

A PLAN FOR THE DELROCK NEIGHBORHOOD

CITY OF DELAVAN
WALWORTH COUNTY
WISCONSIN

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**COMMUNITY ASSISTANCE PLANNING REPORT
NO. 25**

A PLAN FOR THE DELROCK NEIGHBORHOOD

**CITY OF DELAVAN
WALWORTH COUNTY, WISCONSIN**

**Prepared by the
Southeastern Wisconsin Regional Planning Commission
P.O. Box 769
Old Courthouse
916 N. East Avenue
Waukesha, Wisconsin 53187**

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January 1979

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January 15, 1979

The Honorable Elizabeth Supernaw
Mayor of the City of Delavan
City Hall
123 S. Second Street
Delavan, Wisconsin 53115

Dear Mayor Supernaw:

By letter dated January 17, 1973, and by letter dated August 27, 1973, the City of Delavan requested that the staff of the Southeastern Wisconsin Regional Planning Commission prepare a neighborhood development plan for that area of the City of Delavan and Towns of Darien and Delavan known as the "Delrock Neighborhood." This request was consistent with recommendations contained in the adopted regional land use plan that local communities prepare detailed development plans for neighborhood units in urbanizing areas. The Regional Planning Commission has now completed the plan requested and is pleased to transmit this report documenting that plan.

This report presents basic information on the present stage of development of the Delrock Neighborhood area, including information on the existing land use, sanitary sewerage, water supply, and storm water drainage facilities and on the topography and drainage patterns, soils, woodlands and wetlands, and other natural features of the neighborhood area, all of which constitute important considerations in any neighborhood planning effort. Based on the findings of these inventories and on recommended neighborhood development standards, the report sets forth a series of alternative neighborhood development plans and a recommended plan which is consistent with both regional and local development objectives. Upon its adoption by the City Plan Commission, the plan presented in this report is intended to be used by city officials as a point of departure in the making of development decisions affecting the Delrock Neighborhood.

The Regional Planning Commission staff is appreciative of the assistance provided by elected and appointed officials in the city in the preparation of this plan. The Commission staff stands ready upon request to assist the City in presenting the recommended plan documented in this report to the public for review and evaluation prior to local adoption and to assist in subsequent application of the plan over time.

Sincerely,



K. W. Bauer
Executive Director

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Chapter I

INTRODUCTION

The Southeastern Wisconsin Regional Planning Commission since its inception in 1960 has urged local plan commissions to consider the preparation of detailed neighborhood unit development plans as an important means of guiding and shaping urban land use development and redevelopment in the public interest. SEWRPC Planning Guide No. 1, Land Development Guide, published in November 1963, discussed the importance of neighborhood unit planning to the attainment of good residential land subdivision. This guide indicated that effective public regulation of the important process of land subdivision--a process through which much of the form and character of a community are determined--requires the preparation of detailed neighborhood unit development plans. The regional land use plan originally adopted by the Commission in December 1966 more specifically recommended that local plan commissions identify neighborhood units within areas of existing or proposed urban use and prepare detailed plans for the development of these units.

The City of Delavan on April 30, 1973, entered into an agreement with the Regional Planning Commission to assist it in the delineation of neighborhood units as defined in this report, and in the design of precise development plans for certain of those units. The Commission staff, working with the City of Delavan Plan Commission, identified seven neighborhood units for which proper planning could help to meet the development objectives of the City of Delavan.

The purpose of this report is to describe the precise development plan prepared for one of these seven delineated neighborhood units--the Delrock Neighborhood within the Delavan area. The plan suggests future collector and land access street alignments and attendant block configurations and identifies the locations within the neighborhood best suited for institutional, recreational, and commercial use, as well as for various kinds of residential use. The plan recommends areas that should be protected from intensive development for environmental reasons and indicates the need to reserve major drainageway and utility easements.

GENERAL SETTING

The City of Delavan is located in the Southwestern portion of Walworth County and is situated in the west-central portion of U.S. Public Land Survey Township 2 North, Range 16 East and in the east-central portion of U.S. Public Land Survey Township 2 North, Range 15 East. The City is bordered on the north, south, and east by the unincorporated Town of Delavan, and on the west by the unincorporated Town of Darien. Map 1 shows the location of the City of Delavan in the Southeastern Wisconsin Region and the extent of historic urban development.

The City of Delavan has enjoyed a slow but steady growth since 1950. The population of the City increased from 4,007 persons in 1950 to 4,846 persons in 1960, and to 5,526 persons in 1970. The 1978 population of the City is estimated at 5,638. Population forecasts prepared by the Regional Planning Commission indicate that the population of the City may be expected to reach a level of about 7,000 persons by 1985 and about 8,000 persons by the year 2000. The slow but steady growth of the City dictates the conduct of a sound local planning program to provide a basis for development decisionmaking by local officials.

THE NEIGHBORHOOD CONCEPT

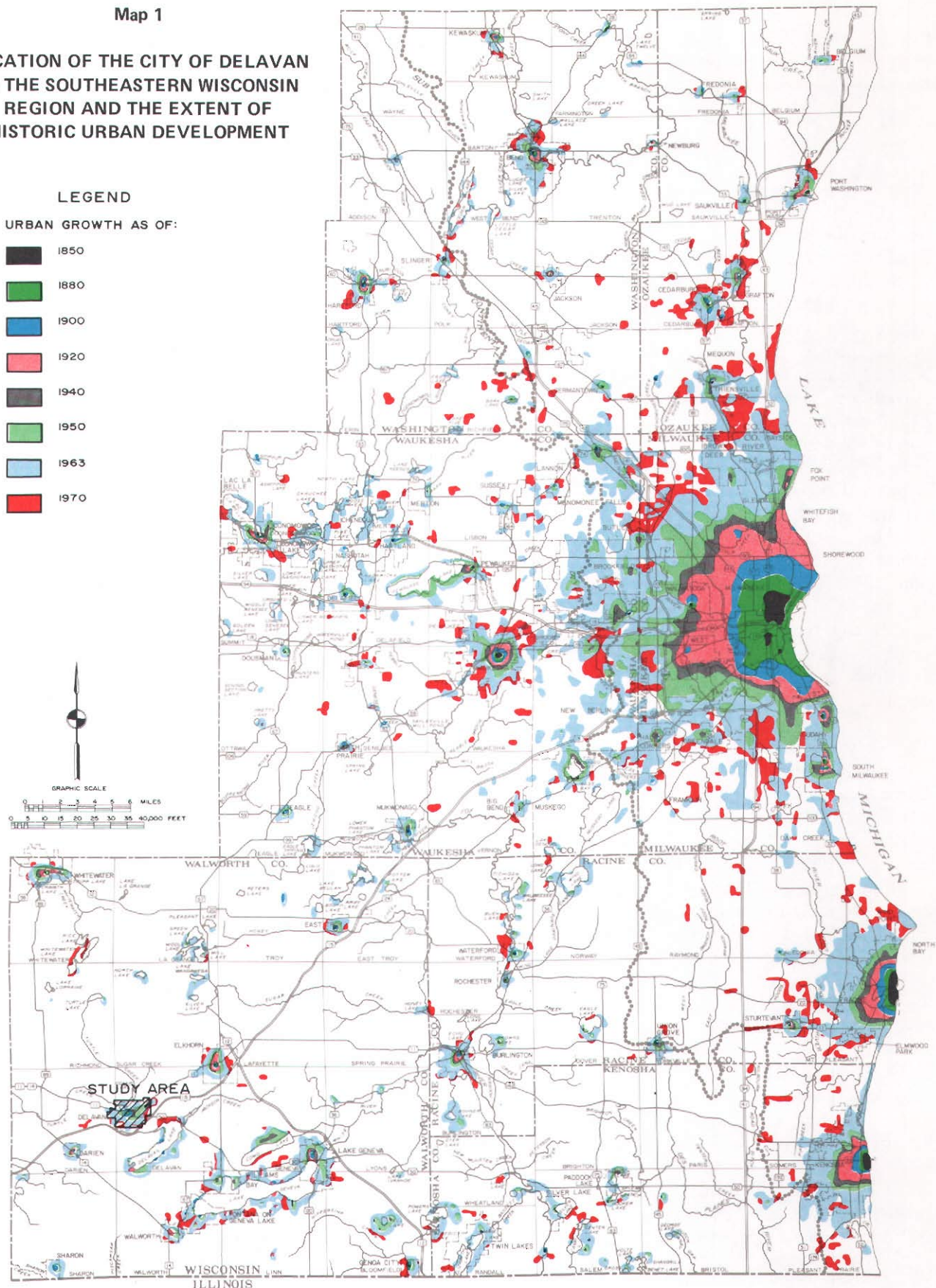
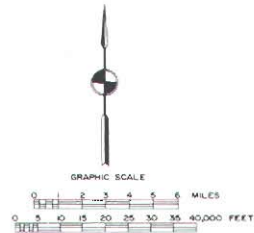
The Regional Planning Commission's recommendation concerning the preparation of detailed neighborhood unit development plans by local plan commissions is based upon the concept that an urban area should be formed of, and developed in, a number of individual cellular units and not of a single, large, formless mass. These cellular units may be categorized by their primary or predominant land use and, as such, may be industrial, commercial, institutional, or residential. The latter type of unit is the concern of this report.

Insofar as possible, each residential neighborhood unit should be bounded by arterial streets; major park, parkway, or institutional lands; bodies of water; or other natural or cultural features which

Map 1

**LOCATION OF THE CITY OF DELAVAN
IN THE SOUTHEASTERN WISCONSIN
REGION AND THE EXTENT OF
HISTORIC URBAN DEVELOPMENT**

LEGEND
URBAN GROWTH AS OF:



Source: SEWRPC.

serve to clearly and physically separate each unit from the surrounding units. Each residential neighborhood unit should provide housing for that population for which, by prevailing local standards, one public elementary school of reasonable size is required. The unit should further provide, within established overall density limitations, a broad range of lot sizes and housing types; a full complement of those public and semipublic facilities needed by the family within the immediate vicinity of its dwelling, such as church, neighborhood park, and neighborhood shopping facilities; and ready access to the arterial street system and, thereby, to those urban activities and services which cannot as a practical matter be provided in the immediate vicinity of all residential development: namely, a major employment center, community and regional shopping centers, major recreational facilities, and major cultural and educational centers. The internal street pattern of the residential neighborhood unit should be designed to facilitate vehicular and pedestrian circulation within the unit, but to discourage penetration of the unit by heavy volumes of fast through traffic. Each residential neighborhood unit should have a central feature or focal point around which the unit is developed to promote a sense of physical unity. In this respect, the elementary school should be located adjacent to the neighborhood park so that the school and park together may function as a neighborhood center and thus provide a focal point of the neighborhood design. The school and park should be located within walking distance of the rest of the neighborhood unit.

The residential neighborhood unit is intended to accommodate safe and healthy family home life, along with the activities associated with it. The neighborhood should be designed to promote stability and the preservation of amenities and should be large enough to maintain and protect its own environment. The neighborhood concept is intended to promote convenience in living and traveling within an urban area; to promote harmony and beauty in urban development; and to bring the living area of the urban family into a scale that allows the individual to feel at home and encourages the individual to take an active part in neighborhood and community affairs. The neighborhood unit concept is also intended to facilitate the difficult task of good land subdivision design. The proper relationship of individual subdivisions to areawide features, to existing and proposed land uses, and to other subdivisions can best be achieved through a precise plan for neighborhood unit development.

The neighborhood unit concept also provides a means for involving citizens in local planning programs. A neighborhood is that area most closely associated with the daily activities of family life, such as elementary education or convenience shopping. Residential neighborhoods, however, depend on the larger community for basic employment, comparison shopping, higher education, cultural activities, and certain personal services. A group of neighborhoods which function as a unit, providing the necessary level of external services and facilities required by the neighborhoods in the group, may be described as a community. By identifying neighborhood units and grouping them into communities, residential areas may be planned to provide a physical environment that is healthy, safe, convenient, and attractive; and public sentiment can be constructively focused on the community of interest so created. Because of its emphasis on the day-to-day needs and concerns of the family, neighborhood planning is particularly "people-oriented."

Unlike the community comprehensive, or master, plan, which is necessarily quite general, the plan developed for a neighborhood is quite precise. It explicitly depicts alternative development patterns which are practicable to meet such needs as traffic circulation, storm water drainage, sanitary sewerage, water supply, and a sound arrangement of land uses. Neighborhood planning, therefore, must involve careful consideration of such factors as soil suitability, land slopes, drainage patterns, flood hazards, and woodland and wetland cover; existing and proposed land uses in and surrounding the neighborhood unit; and real property boundaries. Although the neighborhood unit concept most readily applies to medium- and high-density residential areas, it can be successfully applied in low-density areas with some modifications of the design standards. Table 1 illustrates a typical land use distribution in a medium-density planned neighborhood unit and is intended to provide a basis of comparison for the specific neighborhood unit designs presented herein.

The neighborhood unit development plan, while precise, must nevertheless also be flexible. The plan is intended to be used as a standard for evaluating developmental proposals of private and public agencies. It should not be presumed that private developers cannot present development plans harmonious with sound convenient development standards, nor that any development plans which are privately advanced and at variance in some respect with the adopted neighborhood plan

Table 1

LAND USE DISTRIBUTION IN A TYPICAL MEDIUM-DENSITY NEIGHBORHOOD UNIT

Type of Area	Population and Density		Land Use Allocations	
	Number	Percent of Total	Total Acres	Percent of Total
Residential Area			454.4	71.0
Single-Family Area			416.0	65.0
Population	5,330	85.2		
Residential Acres Per 1,000 Population	76.0	—		
Persons Per Residential Acre	12.8	—		
Number of Dwelling Units	1,615	—		
Dwelling Units Per Residential Acre	3.9	—		
Multiple-Family Area			38.4	6.0
Population	925	14.8		
Residential Acres Per 1,000 Population	41.5	—		
Persons Per Residential Acre	24.1	—		
Number of Dwelling Units	355	—		
Dwelling Units Per Residential Acre	9.2	—		
Public Area			32.0	5.0
Elementary School (K-6) Area			9.6	1.5
Number of Classrooms	20	—		
Total Number of Pupils	500	—		
Public Park Area			16.0	2.5
Other Public and Quasipublic Area			6.4	1.0
Neighborhood Commercial Area			6.4	1.0
Street Area			147.2	23.0
Total	6,255	100.0	640.0	100.0

Source: SEWRPC.

are necessarily unacceptable. Local planning officials should remain receptive to proposed plan changes which can be shown to be better than the adopted plan, yet compatible with the overall objectives for the development of the neighborhood and the community as a whole.

COMMUNITY PLANNING IN THE CITY OF DELAVAN

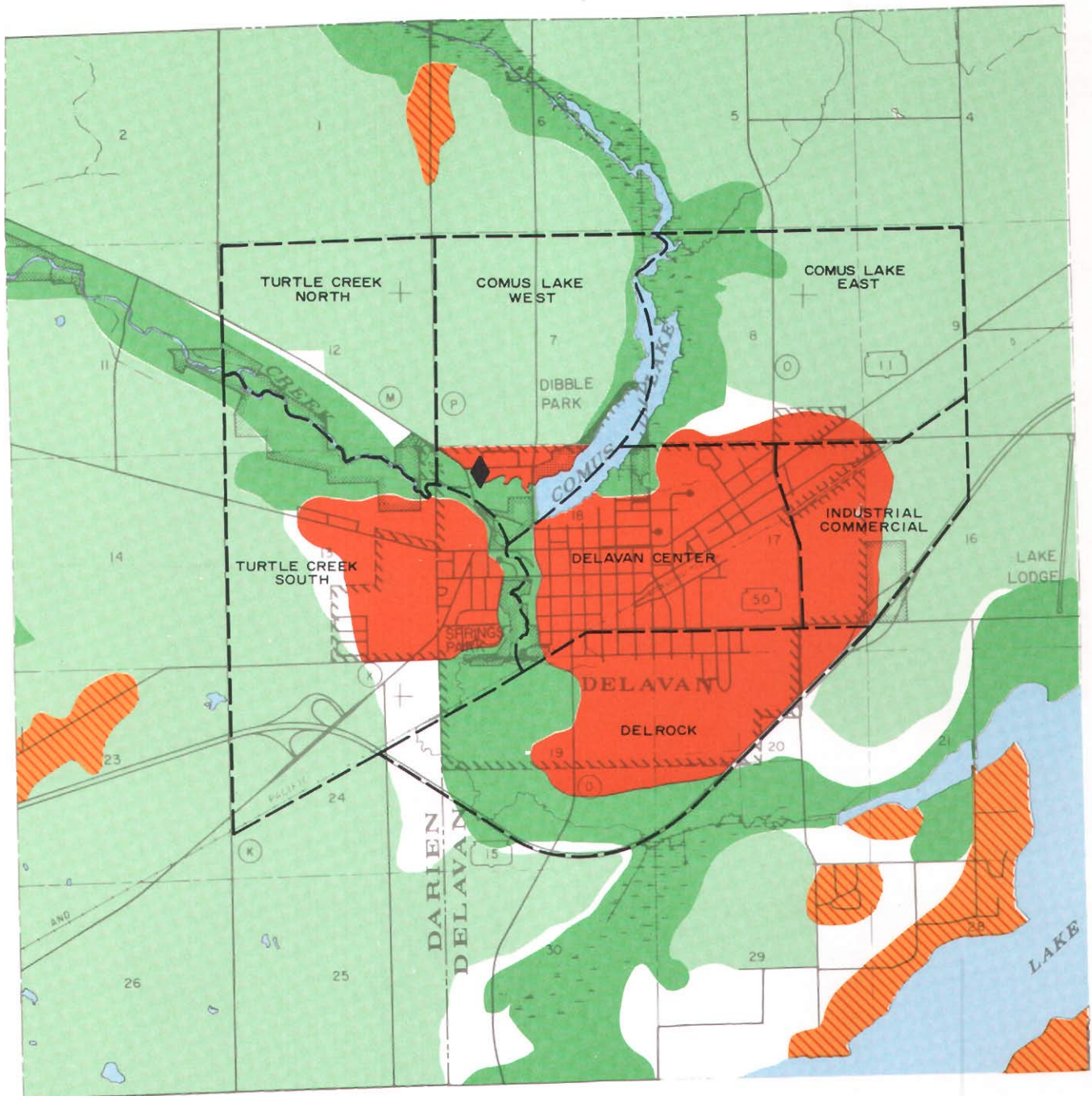
A community should have a comprehensive plan as a basis for the preparation of precise neighborhood unit development plans. Sound planning practice dictates that, just as neighborhood plans should be prepared within the framework of community plans, community plans should be prepared within the framework of regional plans. The adopted regional land use plan as it applies to

the City of Delavan and environs is shown on Map 2, together with recommended neighborhood boundaries.









Several of the adopted regional plan elements are particularly important to the preparation of a general plan for the City of Delavan and, therefore, to the development of precise neighborhood development plans within the City. These elements are described in the following SEWRPC reports: Planning Report No. 15, A Jurisdictional Highway System Plan for Walworth County; Planning Report No. 16, A Regional Sanitary Sewerage System Plan for Southeastern Wisconsin; Planning Report No. 20, A Regional Housing Plan for Southeastern Wisconsin; Planning Report No. 27, A Regional Park and Open Space Plan for Southeastern Wisconsin:2000; and Planning Report No.

Map 2

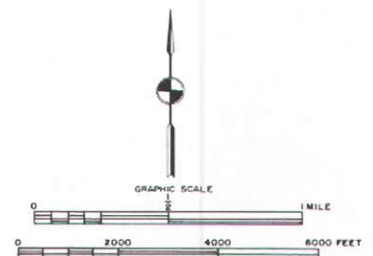
SELECT ELEMENTS OF THE REGIONAL LAND USE, PARK AND OPEN SPACE, AND
TRANSPORTATION PLANS FOR THE CITY OF DELAVAN PLANNING AREA: 2000



LEGEND

- | | | | |
|--|--|---|--------------------------------------|
|  | LOW DENSITY RESIDENTIAL
(0.7-2.2 DWELLING UNITS PER NET
RESIDENTIAL ACRE) |  | PRIME AGRICULTURAL LAND |
|  | MEDIUM DENSITY RESIDENTIAL
(2.3-6.9 DWELLING UNITS PER NET
RESIDENTIAL ACRE) |  | OTHER AGRICULTURAL AND
RURAL LAND |
|  | PUBLIC SEWAGE TREATMENT PLANT |  | WATER |
|  | PRIMARY ENVIRONMENTAL CORRIDOR |  | DELINEATED NEIGHBORHOOD
BOUNDARY |

Source: SEWRPC.



25, A Regional Land Use Plan and a Regional Transportation Plan for Southeastern Wisconsin: 2000. The findings and recommendations of these adopted regional plan elements are reflected in the neighborhood unit development plan presented herein.

In preparation for its overall planning program, the City of Delavan in August 1971 undertook preparation of large-scale (1" = 100' scale, two-foot contour interval) topographic and cadastral maps. The maps and attendant control surveys were completed in accordance with specifications prepared for the City by the Regional Planning Commission and involved the relocation, monumentation, and placement on the Wisconsin State Plane Coordinate System of U. S. Public Land Survey corners within the City and the determination of the grid lengths and bearings of all quarter section lines. The resulting topographic and cadastral information was essential to the conduct of the precise neighborhood development planning program documented herein.

NEIGHBORHOOD DELINEATION

The City of Delavan Plan Commission has identified seven neighborhood units for which detailed neighborhood plans should eventually be developed. These are: the Turtle Creek North, Turtle Creek South, Comus Lake West, Comus Lake East, Delavan Center, Industry/Commercial, and the Delrock neighborhoods. The boundaries of these neighborhoods are shown on Map 2. The seven neighborhoods were delineated based on the concepts described above and the standards set forth in Table 1. The neighborhood boundaries were located along strong environmental or transportation barriers such as the Chicago, Milwaukee, St. Paul & Pacific Railroad right-of-way, Lake

Comus, Turtle Creek, STH 11 and STH 15 and along section lines which in some instances are located along existing roads.

NEIGHBORHOOD LOCATION AND BOUNDARIES

The Delrock Neighborhood, one of seven neighborhoods delineated for the City of Delavan, is located in the south-central portion of Delavan. The neighborhood is bounded on the north by Geneva Street; on the northwest by the Chicago, Milwaukee, St. Paul & Pacific Railroad; and on the southwest, south, and southeast by the Rock Freeway (STH 15). Also, the Delrock Neighborhood is bounded on the northwest by the delineated Turtle Creek South Neighborhood, and on the north by the Delavan Center and the Industry/Commercial Neighborhoods. The area of the Delrock Neighborhood is approximately 946 acres. Of the total area of the neighborhood, 556 acres, or 59 percent, lie within the City of Delavan; 351 acres, or 37 percent, within the Town of Delavan; and 39 acres, or 4 percent, within the Town of Darien.

HISTORY OF THE DELROCK NEIGHBORHOOD

Development of the area now known as the Delrock Neighborhood began as early as 1841 with the original plat of Delavan, a portion of which is located in the northern part of the Delrock Neighborhood. The development of the S. A. Phoenix Addition was begun in 1889. No further platting of land in the neighborhood took place until the platting of the Fairview Heights Addition in 1950. In 1953 the Allynhurst Subdivision and the South Park Subdivision were platted. The most recent platting activity in the neighborhood was the Todd Miller Subdivision of 1977.

Chapter II

INVENTORY FINDINGS AND ANALYSIS

INTRODUCTION

Reliable basic planning and engineering data are essential to the formulation of workable development plans. Consequently, inventory becomes the first operational step in any planning process. Factual information is particularly crucial to good neighborhood planning because of the precision of the plan to be produced. The second step, the formulation of a neighborhood plan, requires that factual data be developed on existing characteristics of the neighborhood area including topography and surface drainage, the extent of woodlands and wetlands, existing land use, real property ownership, community utilities and facilities, street and highway facilities, and soils.

TOPOGRAPHY AND SURFACE DRAINAGE

Map 3 shows the topography and drainage pattern of the Delrock Neighborhood area. The area consists of gently rolling terrain with local relief of approximately 20 feet. A low ridge is located through the southern portion of the neighborhood area, extending in a generally east-west direction. Land has been developed in a strip pattern along the south side of Geneva Street. Development has also occurred along CTH 0 in a linear pattern. A large area extending in an east-west direction through the center of the neighborhood area acts as a drainage swale to direct storm water to Swan Creek, which extends along the southwestern and northwestern boundaries of the neighborhood.

The Delrock Neighborhood is located within the Rock River watershed. Map 3 shows the location of the various subbasin boundaries, together with the directions of storm water flow. The subbasins located in the southern portions of the Delrock Neighborhood flow to Swan Creek. The subbasins located in the northern portion of the neighborhood discharge to Swan Creek and Turtle Creek.

SOILS

Soils in the Delrock Neighborhood are generally well suited to urban development. Table 2 and Map 4 indicate that approximately 122 acres, or about

13 percent of the Delrock Neighborhood, are covered with soils which have limitations which should be carefully considered in the development of the neighborhood. Generally, soils with severe limitations for urban development due to a fluctuating or high water table or ponding, overflow, runoff, or overwash hazards are found in the southern portion of the neighborhood.

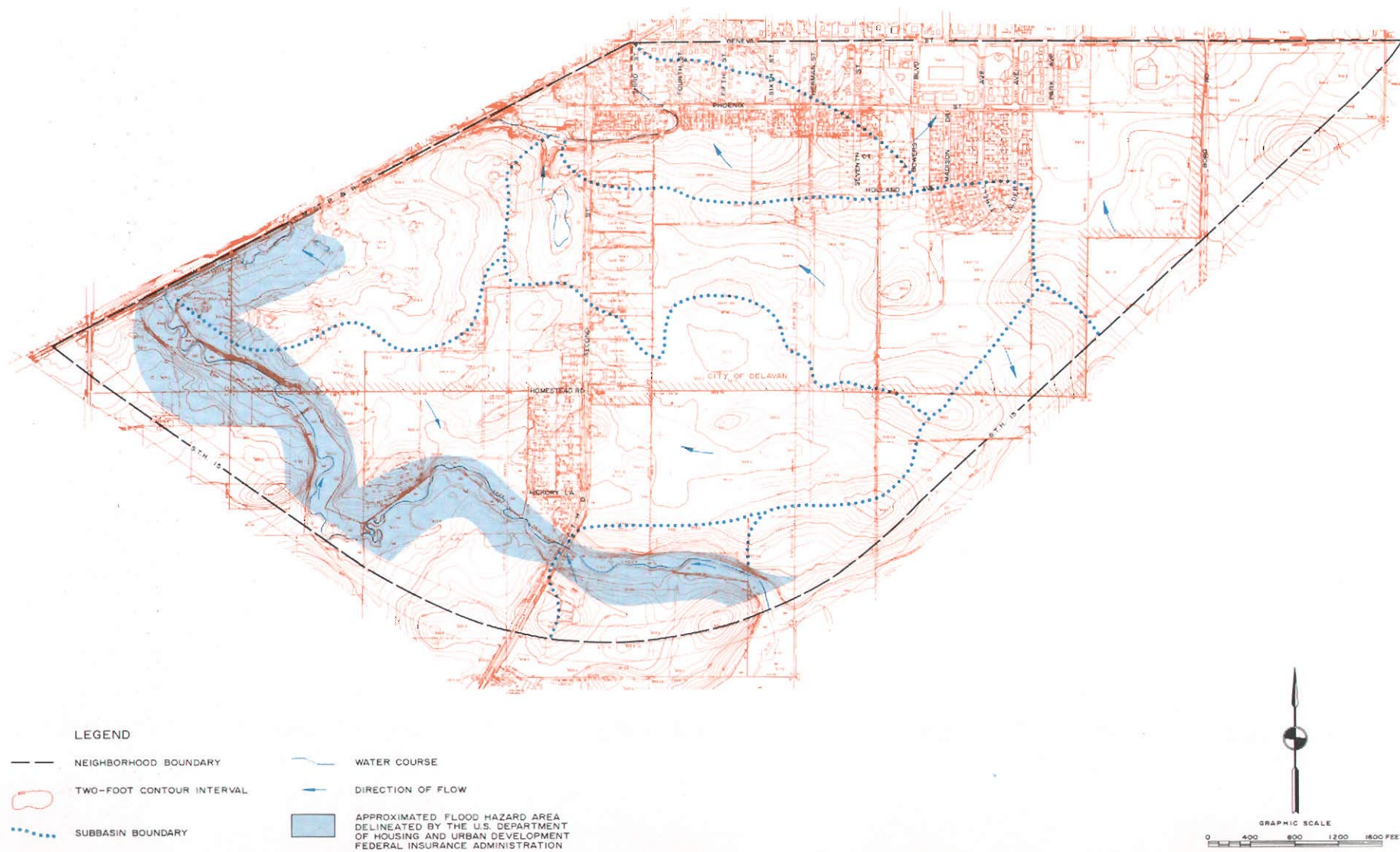
Soils which occur in the neighborhood area and which have severe or very severe limitations for residential development on lots served by public sanitary sewerage facilities are listed in Table 3 and shown, with respect to location and extent, on Map 5. Wet alluvial soils represent about one-half of the area covered by soils having severe limitations for urban development with sanitary sewer service, and about 5 percent of the total area of the neighborhood. The wet alluvial soils are characterized by a high water table and frequent overflow. Map 6 shows the location and extent of the soils which have severe or very severe limitations for residential development without public sewers on lots one acre or more in area.

Table 3 lists all of the soils found in the neighborhood area and summarizes the suitability of these soils for a variety of urban land uses. Seventeen identified types of soils occur within the neighborhood area, with the most prevalent type being the McHenry silt loam, which covers nearly 30 percent of the total area of the neighborhood. The table lists the potential developmental problems associated with particular land uses on each soil type in the Delrock Neighborhood.

WOODLANDS AND WETLANDS

Areas of wetlands are located in the southern and western portions of the neighborhood along Swan Creek. The wetlands occupy 62 acres, or 6.5 percent of the total neighborhood area. Woodlands are almost nonexistent in the neighborhood. Only very small scattered groupings of trees are found, and most of these are associated with plantings for existing development.

TOPOGRAPHY AND SURFACE DRAINAGE PATTERNS FOR THE DELROCK NEIGHBORHOOD: 1978



Source: U. S. Department of Housing and Urban Development, Federal Insurance Administration; and SEWRPC.

Table 2

SELECT CHARACTERISTICS OF SOILS IN THE DELROCK NEIGHBORHOOD

Selected Characteristics	Area Covered in Acres	Percent of Total
Swamps, marshes, organic materials, or soils subject to flooding or overflow	62	6.6
Soils that have a fluctuating or high water table or that are subject to a ponding, overflow, runoff, or overwash hazard	53	5.6
Soils that have a slow permeability rate	0	0
Soils that are underlain by shallow bedrock or in which filter fields are subject to siltation or the groundwater table is subject to contamination	0	0
Lands having a slope of 12 percent or greater and where soils may be erosive	7	0.7
All other soils	824	87.1
Total	946	100.0

Source: SEWRPC.

EXISTING LAND USE

The existing land uses within the Delrock Neighborhood as of 1975 are set forth in summary form in Table 4 and on Map 7. Agricultural and open and unused lands account for about 63 percent of the total area, while residential lands account for 10 percent. Existing urban development, as already noted, is located along Geneva Street in the northern portion of the neighborhood and along CTH 0 in the western portion of the neighborhood. A significant land use in the neighborhood is the 127 acres devoted to the Delrock Golf Course.

LAND USE CONTROL

Land use development within that part of the neighborhood located in the City of Delavan is regulated by the City of Delavan Zoning Ordinance. Seven of the 10 zoning districts provided in the city ordinance have been applied within the neighborhood. Five of the 26 zoning districts in the Walworth County Zoning Ordinance have been applied within that portion of the neighborhood lying in the Town of Delavan, and 3 of the 26 districts provided in the county ordinance have been applied within that part of the neighborhood lying in the Town of Darien. The boundaries of these zoning districts, together with the corporate

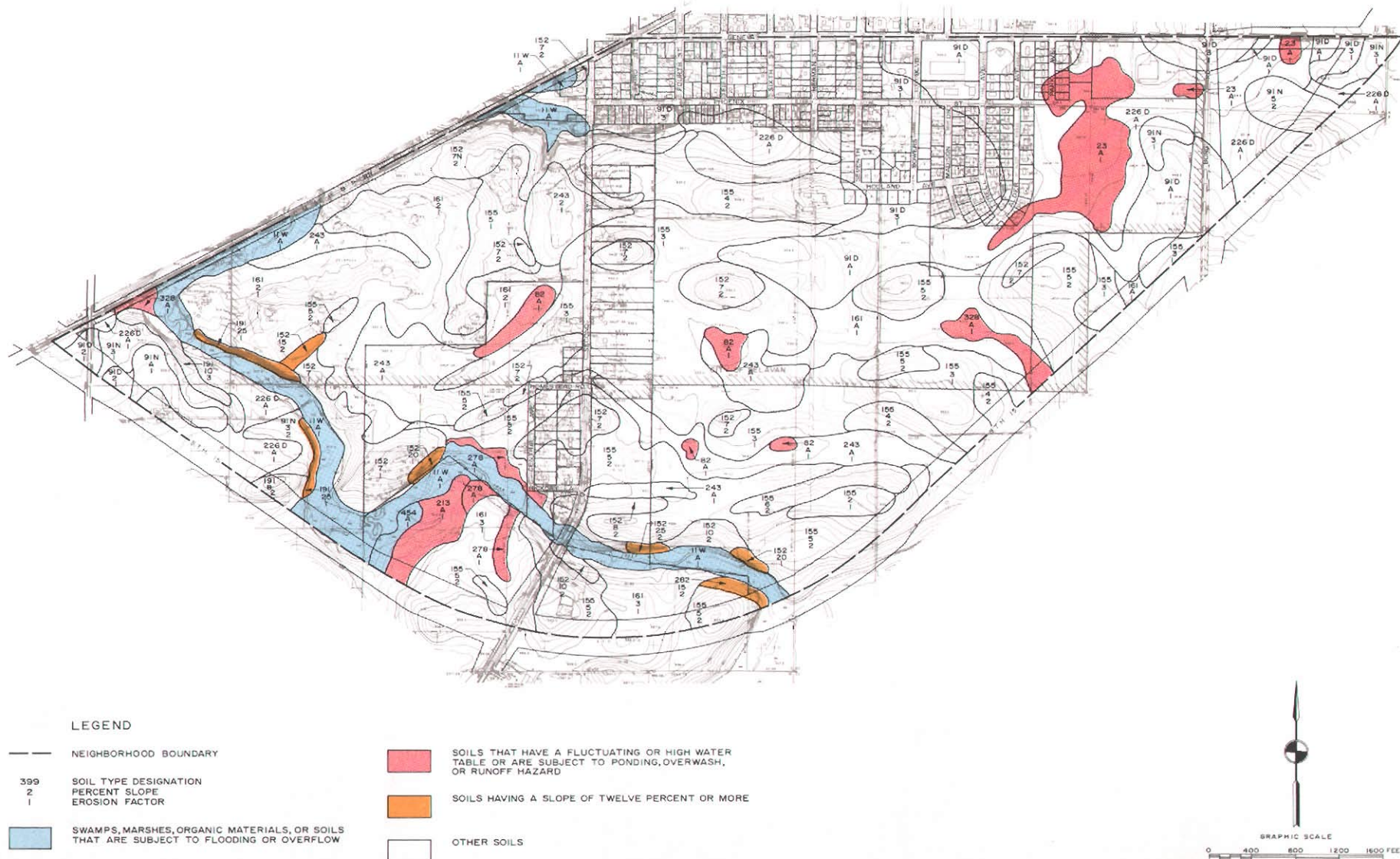
limit lines, are shown on Map 8. Pertinent information concerning the regulations governing each of these 14 zoning districts is set forth in Table 5. Approximately 62 percent of the Delrock Neighborhood is currently zoned for residential use. The recommended neighborhood unit plan presented herein is intended to provide a basis for rezoning the neighborhood unit into zoning districts which are more suitable to achieving the regional and local development objectives as expressed in the recommended plan.

COMMUNITY UTILITIES

In 1978 237 acres of the neighborhood or about 68 percent of the existing urban development within the neighborhood and 25 percent of the total area of the neighborhood were served by public sanitary sewer facilities as shown on Map 9. Sanitary sewer facilities have not been expanded into the southernmost areas of the neighborhood. In 1978 189 acres of the neighborhood, or about 54 percent of the existing urban development within the neighborhood and 20 percent of the total area of the neighborhood, were served by public water supply facilities as shown on Map 10. In 1978 112 acres of the neighborhood, or about 32 percent of the existing urban development within the neighborhood and 12 percent of the

Map 4

SELECT CHARACTERISTICS OF SOILS IN THE DELROCK NEIGHBORHOOD



Source: SEWRPC.

Table 3

**LIMITATIONS OF SOILS FOR RESIDENTIAL, LIGHT INDUSTRIAL, AND COMMERCIAL
DEVELOPMENT FOR THOSE SOIL SERIES FOUND IN THE DELROCK NEIGHBORHOOD**

Soil Number and Soil Name	Soil Limitations				Area Covered in Acres	Percent of Neighborhood
	Residential Development with Public Sewer Service	Onsite Soil Absorption Sewage Disposal Systems		Light Industrial and Commercial Buildings		
		Lots Less than One Acre	Lots One Acre or More			
11W Alluvial Land, Wet	Very severe—high water table; frequent overflow.	Very severe—high water table; frequent overflow	Very severe—high water table; frequent overflow	Very severe—high water table; frequent overflow	51	5.4
23 Lawson Silt Loam	Very severe—low bearing capacity; frost heave; frequent overflow; high water table.	Very severe—systems will not operate when flooded	Very severe—systems will not operate when flooded	Very severe—frequent overflow; high water table; piping; low	23	2.4
82 Juneau Silt Loam	Moderate—erosive on slopes; low bearing capacity; frost heave; occasional overflow.	Severe—systems will not operate when flooded	Severe—systems will not operate when flooded	Severe—occasional overflow; low bearing capacity; piping; frost heave	8	0.8
91D and 91N Parr Silt Loam	Very slight on 0-6 percent slopes and slight on 6-12 percent slopes; erosive on slopes.	Slight on 0-6 percent slopes; moderate on 6-12 percent slopes	Slight on 0-6 percent slopes; moderate on 6-12 percent slopes	Slight on 0-6 percent slopes; moderate on 6-12 percent slopes; erosive on slopes	205	21.7
152 Lapeer Loam, Shallow Variant	Slight on 0-12 percent slopes; moderate on 12-20 percent slopes; severe on steeper slopes; stony in places.	Slight on 0-6 percent slopes; moderate on 6-12 percent slopes; severe on steeper slopes	Slight on 0-6 percent slopes; moderate on 6-12 percent slopes; severe on steeper slopes	Slight on 0-6 percent slopes; moderate on 6-12 percent slopes; severe on steeper slopes; erosive on slopes	103	10.9
155 McHenry Silt Loam	Very slight on 0-6 percent slopes; slight on 6-12 percent slopes; moderate on 12-20 percent slopes; severe on steeper slopes; erosive on slopes; frost heave.	Slight on 0-6 percent slopes; moderate on 6-12 percent slopes; severe on steeper slopes	Slight on 0-6 percent slopes; moderate on 6-12 percent slopes; severe on steeper slopes	Slight on 0-6 percent slopes; moderate on 6-12 percent slopes; severe on steeper slopes; erosive on slopes; subject to frost heave	266	28.2
161 Dodge Silt Loam	Very slight on 0-6 percent slopes; slight on 6-12 percent slopes; erosive on slopes; frost heave.	Slight on 0-6 percent slopes; moderate on 6-12 percent slopes	Slight on 0-6 percent slopes; moderate on 6-12 percent slopes	Slight on 0-6 percent slopes; moderate on 6-12 percent slopes; erosive on slopes; frost heave	106	11.2
191 Parr Silt Loam, Shallow Variant	Very slight on 0-6 percent slopes; slight on 6-12 percent slopes; moderate on 12-20 percent slopes; severe on steeper slopes; erosive on slopes.	Slight on 0-6 percent slopes; moderate on 6-12 percent slopes; severe on steeper slopes	Slight on 0-6 percent slopes; moderate on 6-12 percent slopes; severe on steeper slopes	Slight on 0-6 percent slopes; moderate on 6-12 percent slopes; severe on steeper slopes; erosive on slopes	12	1.3
213 Ehler Silt Loam	Severe—liquefies easily; low bearing capacity; frost heave; high water table; wet basements; floatation of pipes.	Very severe—high water table; systems will not operate	Very severe—high water table; systems will not operate	Severe—high water table; high shrink-swell potential; piping	5	0.5
226D Keyser Silt Loam	Slight—erosive on slopes; frost heave.	Moderate—high water table; systems will not operate	Moderate—high water table; systems will not operate	Slight on 0-6 percent slopes; frost heave	58	6.1
243 Calamus Silt Loam	Slight—erosive on slopes; frost heave.	Moderate—high water table for short periods restricts use of systems	Moderate—high water table for short periods restricts use of systems	Slight on 0-6 percent slopes; moderate on 6-12 percent slopes; erosive on slopes; high water table for short periods; frost heave	78	8.2
278 Clyman Silt Loam	Moderate—frost heave; high water table.	Very severe—high water table; systems will not operate	Severe—high water table; systems will not operate	Moderate—high water table; frost heave	3	0.3
282 Cosco-Rodman Loams	Moderate on 0-12 percent slopes; severe on steeper slopes; erosive on slopes; droughty; difficult to install utilities; stony in places.	Moderate on 0-12 percent slopes; severe on steeper slopes; contamination of groundwater	Moderate on 0-12 percent slopes; severe on steeper slopes; contamination of groundwater	Slight on 0-6 percent slopes; moderate on 6-12 percent slopes; severe on steeper slopes; erosive on slopes; stony in places	3	0.3
328 Pistakee Silt Loam	Severe—low bearing capacity, frost heave; high water table; occasional overflow.	Very severe—high water table; systems will not operate	Very severe—high water table; systems will not operate	Severe—high water table; low bearing capacity	14	1.5
450 and 454 Houghton Mucky Peat	Very severe—erosive; subject to shrinkage; low bearing capacity; high water table.	Very severe—high water table; systems will not operate	Very severe—high water table; systems will not operate	Very severe—erosive; high compressibility and instability; high water table	11	1.2
Total					946	100.0

Source: SEWRPC.

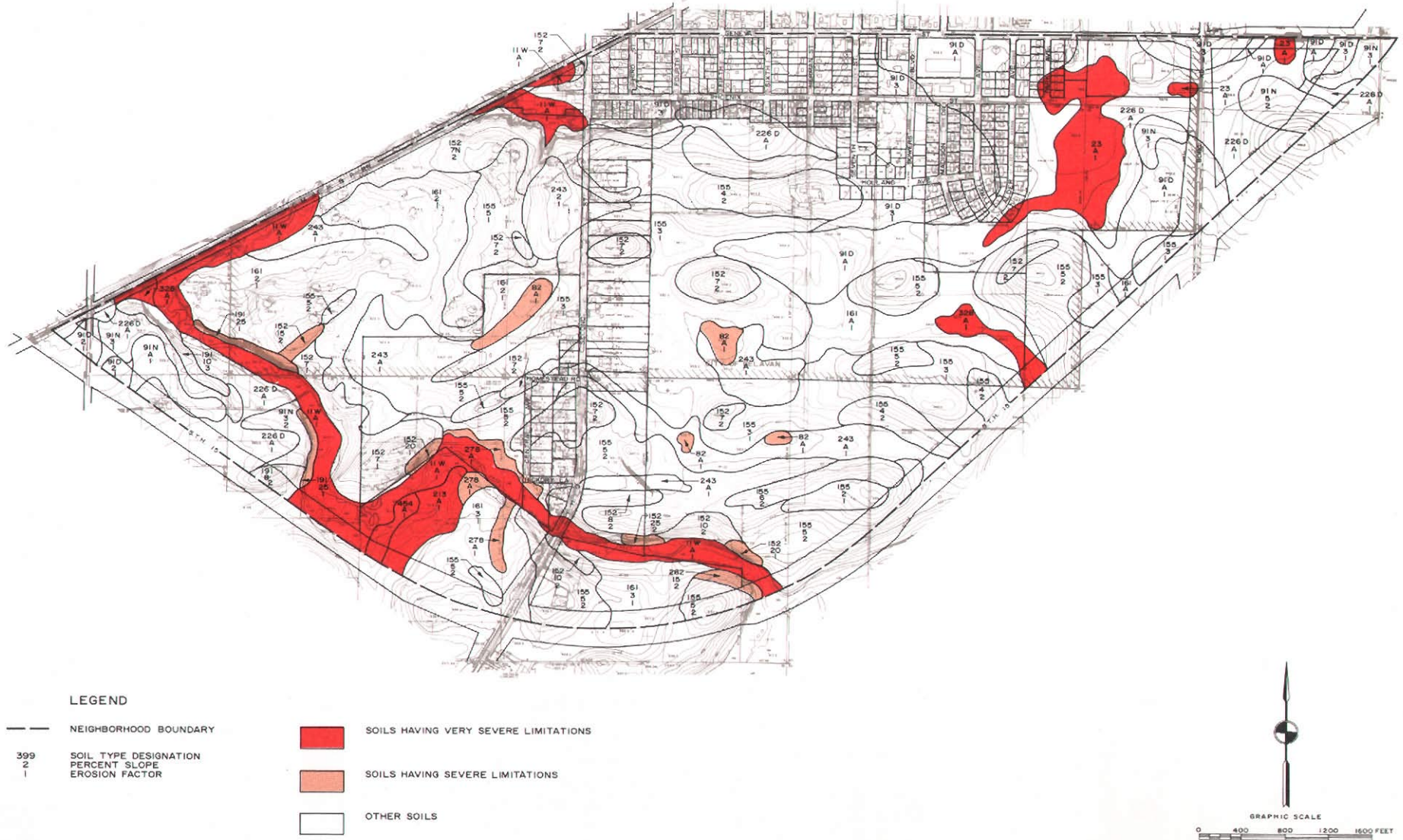
SOIL LIMITATIONS FOR RESIDENTIAL DEVELOPMENT ON LOTS SERVED BY PUBLIC SEWER SERVICE IN THE DELROCK NEIGHBORHOOD



Source: SEWRPC.

Map 6

SOIL LIMITATIONS FOR RESIDENTIAL DEVELOPMENT ON LOTS ONE ACRE OR MORE
IN AREA SERVED BY ONSITE ABSORPTION SEWAGE DISPOSAL SYSTEMS



Source: SEWRPC.

Table 4

EXISTING LAND USE IN THE DELROCK NEIGHBORHOOD: 1975

Land Use Category	Number of Acres	Percent of Neighborhood
Residential		
Single-Family	91.2	9.6
Two-Family	1.1	0.1
Multiple-Family	3.9	0.4
Subtotal	96.2	10.1
Commercial		
Neighborhood Retail and Service	—	—
Community Service	15.0	1.6
Subtotal	15.0	1.6
Industrial	4.6	0.5
Governmental/Institutional		
Public	—	—
Private	7.2	0.8
Subtotal	7.2	0.8
Park and Recreational		
Neighborhood Parks	—	—
Community Parks	—	—
Other Recreational	118.7	12.5
Subtotal	118.7	12.5
Transportation and Utilities		
Arterial Streets	68.0	7.2
Collector Streets	10.4	1.1
Minor Land Access Streets	22.0	2.3
Railroad Right-of-Way	6.4	0.7
Utility Easements	—	—
Subtotal	106.8	11.3
Agricultural, Open and Unused Lands . . .	597.5	63.2
Total	946.0	100.0

Source: SEWRPC.

total area of the neighborhood were served by storm sewer facilities as shown on Map 11. One small area along 7th Street north of 7th Court is served by a short storm sewer pipe but is not part of the City Storm Sewer System.

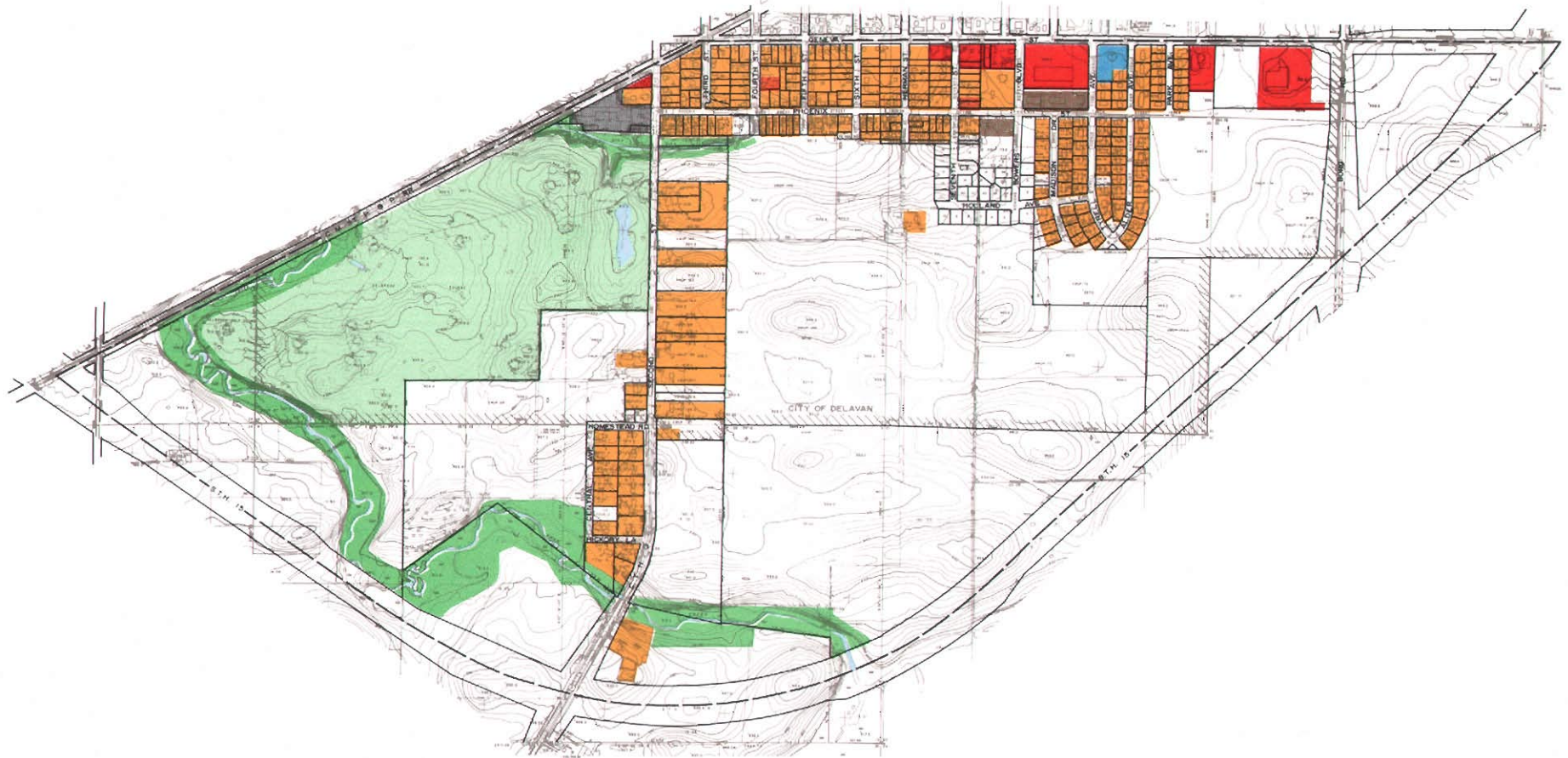
COMMUNITY FACILITIES

As already noted, located within the delineated Delrock Neighborhood boundaries is the Delrock Golf Course, a nonpublic, commercial golf course

127 acres in size. Presently no schools or other public community facilities are located within the neighborhood. The area is presently served by Delavan-Darien High School located on the west side of Delavan approximately one mile away which provides, in addition to secondary educational services, baseball diamonds, a playfield, and tennis courts; by Phoenix Elementary School, located approximately three-fourths of a mile away on the west side of Delavan which provides, in addition to primary educational services, basketball

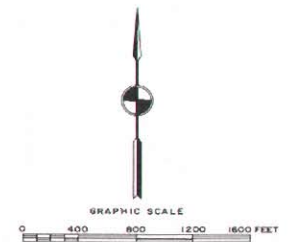
Map 7

EXISTING LAND USE IN THE DELROCK NEIGHBORHOOD: 1975



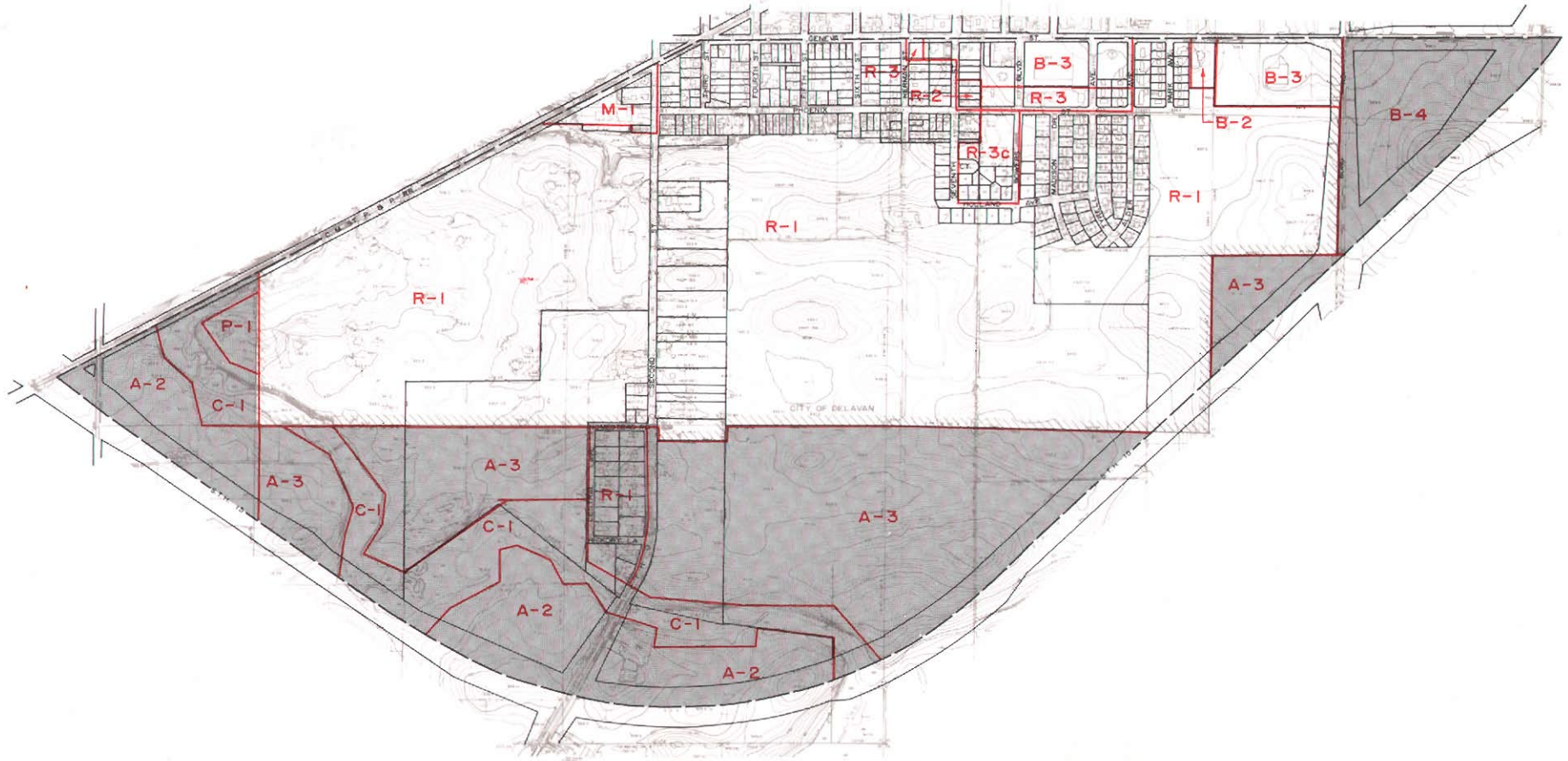
LEGEND

	NEIGHBORHOOD BOUNDARY		COMMERCIAL		SPECIAL PURPOSE RECREATION SITE (GOLF COURSE)
	PROPERTY BOUNDARY LINE		INDUSTRIAL		WATER
	SINGLE-FAMILY RESIDENTIAL		GOVERNMENTAL AND INSTITUTIONAL		AGRICULTURAL AND OTHER OPEN LANDS
	TWO-FAMILY RESIDENTIAL		WETLANDS		
	MULTIPLE-FAMILY				



Map 8

EXISTING ZONING DISTRICTS IN THE DELROCK NEIGHBORHOOD: 1978



LEGEND

- NEIGHBORHOOD BOUNDARY
- PROPERTY BOUNDARY LINE
- ZONING DISTRICT BOUNDARY

CITY OF DELAVAN

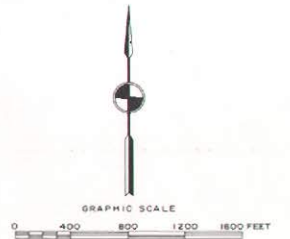
- R-1 RESIDENTIAL DISTRICT - SINGLE-FAMILY
- R-2 RESIDENTIAL DISTRICT - MULTIPLE-FAMILY
- R-3 RESIDENTIAL DISTRICT - APARTMENT

- R-3c RESIDENTIAL DISTRICT - CONDOMINIUM
- B-2 BUSINESS DISTRICT - OFFICE
- B-3 BUSINESS DISTRICT - SERVICE AND WHOLESALE
- M-1 MANUFACTURING DISTRICT - LIMITED

TOWN OF DELAVAN AND TOWN OF DARIEN

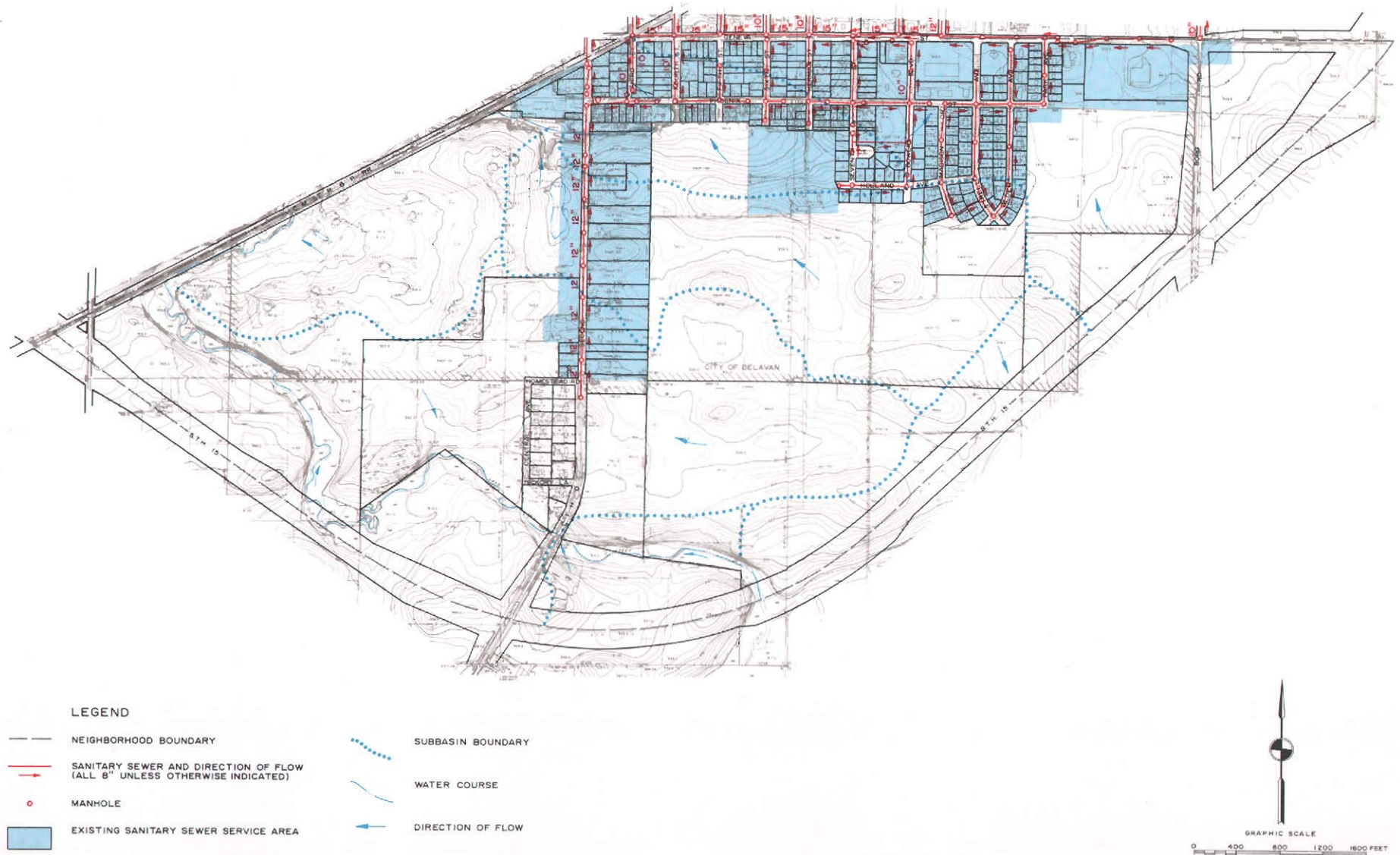
- R-1 SINGLE-FAMILY RESIDENCE DISTRICT

- A-2 AGRICULTURAL DISTRICT
- A-3 AGRICULTURAL DISTRICT
- B-4 BUSINESS DISTRICT
- C-1 CONSERVANCY DISTRICT
- P-1 PARK DISTRICT



Map 9

EXISTING SANITARY SEWER SERVICE IN THE DELROCK NEIGHBORHOOD: 1978



Source: SEWRPC.

Table 5

EXISTING CITY OF DELAVAN ZONING DISTRICTS AS APPLIED IN THE DELROCK NEIGHBORHOOD: 1978

Zoning District	Permitted Uses		Conditional and Special Uses	Maximum Residential Density (dwelling units per net acre)	Minimum Lot Size			Minimum Yard Requirements			Maximum Building Height (feet)	Area of Neighborhood in District (Acres)	Percent of Total
	Principal	Accessory			Total Area (square feet)	Area per Family (square feet)	Width at Setback (feet)	Front Yard (feet)	Side Yard (feet)	Rear Yard (feet)			
R-1 One-Family Dwelling District	One-family detached dwellings	Home occupations	Airport, cemetery, golf courses, hospitals, municipal recreation buildings, PUD, other municipal services	5.4	8,000	8,000	66	25	10	25	35	560	59.2
R-2 Multiple-Family Dwelling District	Single-family dwellings, multiple-family dwellings	Transitional uses	Public buildings, rest homes	21.7	8,000	2,000	66	25	10	25	35	1	0.1
R-3 Apartment Dwelling District	Multiple-family dwellings	—	Public buildings, rest homes	17.4	10,000	2,500	66	25	20	30	50	7	0.7
R-3c Condominium Dwelling District	Condominiums	—	Public buildings	17.4	10,000	2,500	66	25	20	30	50	9	1.0
B-2 Office Business District	Business offices, professional offices, beauty shops, funeral homes, multiple-family dwellings	—	—	N/A	N/A	N/A	N/A	25	10	25	N/A	2	0.2
B-3 Business and Wholesale District	Agricultural Implements sales, auto sales, garages, motels, pet shops, greenhouses	Uses incidental to principal uses	Planned business developments	N/A	N/A	N/A	N/A	25	10	25	45	28	3.0
B-4 Highway Business District	All uses are conditional	—	Auto service, taverns, service stations, hotels, restaurant, animal hospitals, nursing homes	N/A	Sewered Lots Sufficient area for principal and accessory structures, offstreet parking, and loading areas Unsewered Lots 20,000 to 40,000 (depending on soils)	N/A	N/A	N/A	40	40	35	36	3.8

and softball facilities; and Wileman Elementary School located on the east side of Delavan near the boundary of the Delrock Neighborhood which provides, in addition to primary educational services, basketball, picnic area, playground, and softball facilities; Springs Park, a 36-acre city park located less than a quarter mile north of the Delrock Neighborhood which provides a playfield and softball diamond facilities; Phoenix Park, a four-acre city park located one-quarter mile away

in central Delavan, which provides basketball, playground, and softball diamond facilities; and Litmer Park, a one-acre city park located outside the Delrock Neighborhood in the northeast portion of the city which provides tennis courts for recreational use. These facilities, however, are not adequate to serve the Delrock Neighborhood when fully developed, and provisions should be made within the delineated neighborhood to provide elementary school and neighborhood park facilities.

Table 5 (continued)

**EXISTING WALWORTH COUNTY ZONING DISTRICTS AS APPLIED IN THE TOWN OF DARIEN
PORTION OF THE DELROCK NEIGHBORHOOD: 1978**

Zoning District	Permitted Uses		Conditional and Special Uses	Maximum Residential Density (dwelling units per net acre)	Minimum Lot Size			Minimum Yard Requirements			Maximum Building Height (feet)	Area of Neighborhood in District (Acres)	Percent of Total
	Principal	Accessory			Total Area (square feet)	Area per Family (square feet)	Width at Setback (feet)	Front Yard (feet)	Side Yard (feet)	Rear Yard (feet)			
A-2 Agricultural Land District	Apiculture, dairy, grazing, orchards, plant nurseries, vegetable raising	—	Housing for farm laborers, ski hills, government, utilities	N/A	5 acres	5 acres	300	25 to 85	20	100	Farm Dwelling 35 Other Twice the distance to nearest lot line	21.5	2.3
C-1 Lowland Resource Conservation District	Farming, boat landing sites, fish hatcheries, hunting, fishing, swimming, hiking	—	Golf courses, hunting and fishing clubs, campgrounds, utilities	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	12	1.3
M-1 Manufacturing District, Limited	Auto painting, carpet manufacture, plastic products	—	Planned Industrial development, airports, outdoor theaters, auditoriums	N/A	N/A	N/A	N/A	25	25	30	45	6	0.6

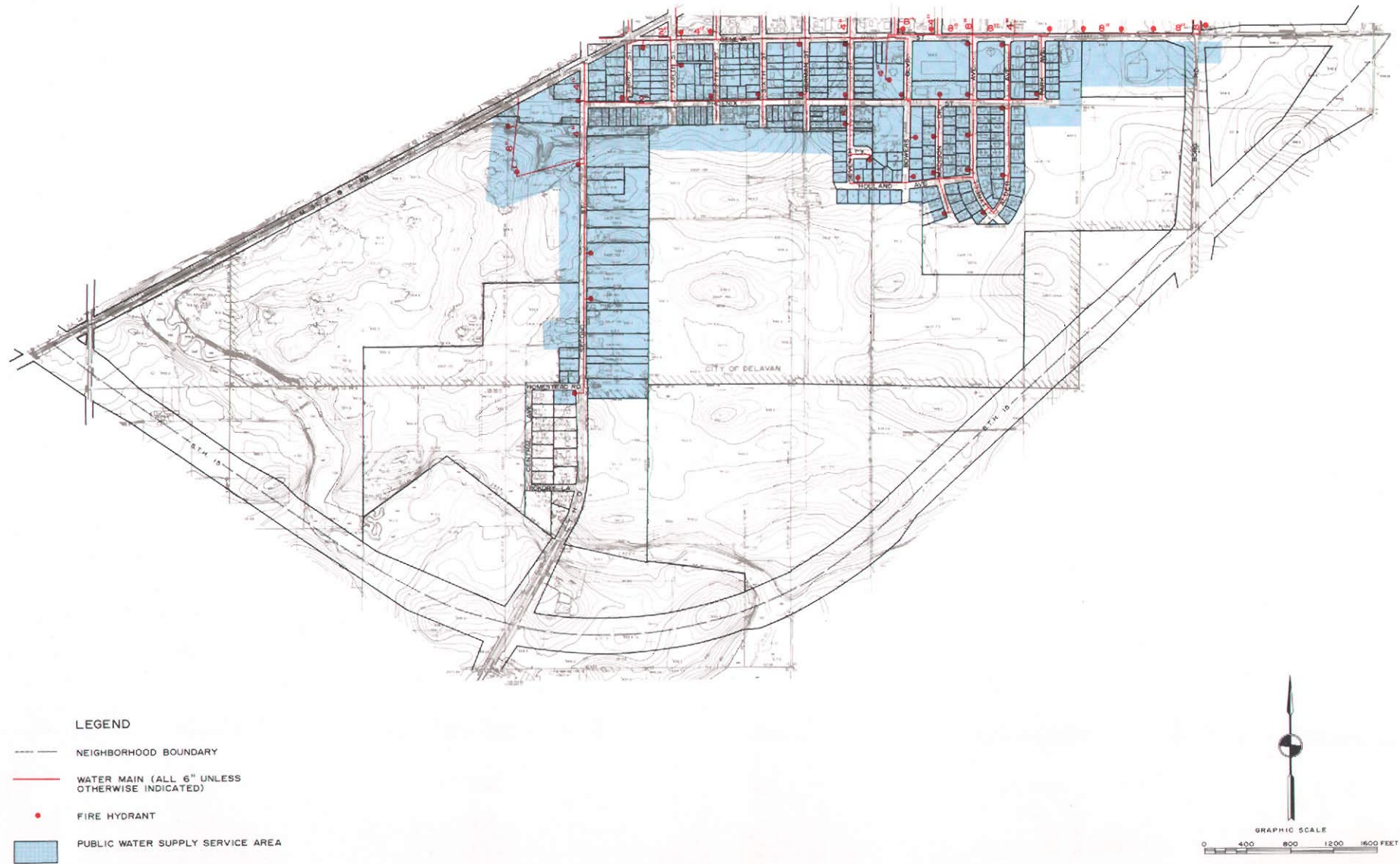
**EXISTING WALWORTH COUNTY ZONING DISTRICTS AS APPLIED IN THE TOWN OF DELAVAN
PORTION OF THE DELROCK NEIGHBORHOOD: 1978**

Zoning District	Permitted Uses		Conditional and Special Uses	Maximum Residential Density (dwelling units per net acre)	Minimum Lot Size			Minimum Yard Requirements			Maximum Building Height (feet)	Area of Neighborhood in District (Acres)	Percent of Total
	Principal	Accessory			Total Area (square feet)	Area per Family (square feet)	Width at Setback (feet)	Front Yard (feet)	Side Yard (feet)	Rear Yard (feet)			
A-2 Agricultural Land District	One single-family farm dwelling, apiculture, dairy, grazing, orchards, plant nurseries, vegetable raising	—	Housing for farm laborers, commercial, feed lot, fur farms, government, utilities	N/A	5 acres	5 acres	300	N/A	20	100	Farm Dwelling 35 Other Twice the distance to nearest lot line	49	5.2
A-3 Agricultural Land Holding District	Apiculture, dairy, grazing, orchards, plant nurseries, vegetable raising	—	Housing for farm laborers, livestock sales, ski hills, fur farms, government, utilities	N/A	35 acres	35 acres	N/A	N/A	20	100	Farm Dwelling 35 Other Twice the distance to nearest lot line	154	16.3
R-1 Single-Family Residence District (unsewered)	Single-family dwellings, parks and playgrounds	—	Golf courses, planned unit developments, home occupations, government, utilities, schools	2.1	20,000 to 40,000 (depending on soils)	20,000 to 40,000 (depending on soils)	100 to 150 (depending on soils)	N/A	15	25	35	13	1.4
C-1 Lowland Resource Conservation District	Farming, boat landing sites, fish hatcheries, hunting, fishing	—	Golf courses, hunting and fishing clubs, campgrounds, utilities	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	43	4.5
P-1 Recreational Park District	Parks, forest reserves, golf courses, picnic grounds	—	Golf courses, ski hills, riding stables, sports fields, firearm ranges, utilities	N/A	N/A	N/A	N/A	N/A	50	50	35	4.5	0.4
Total												946	100.0

N/A indicates data not applicable.

Source: SEWRPC.

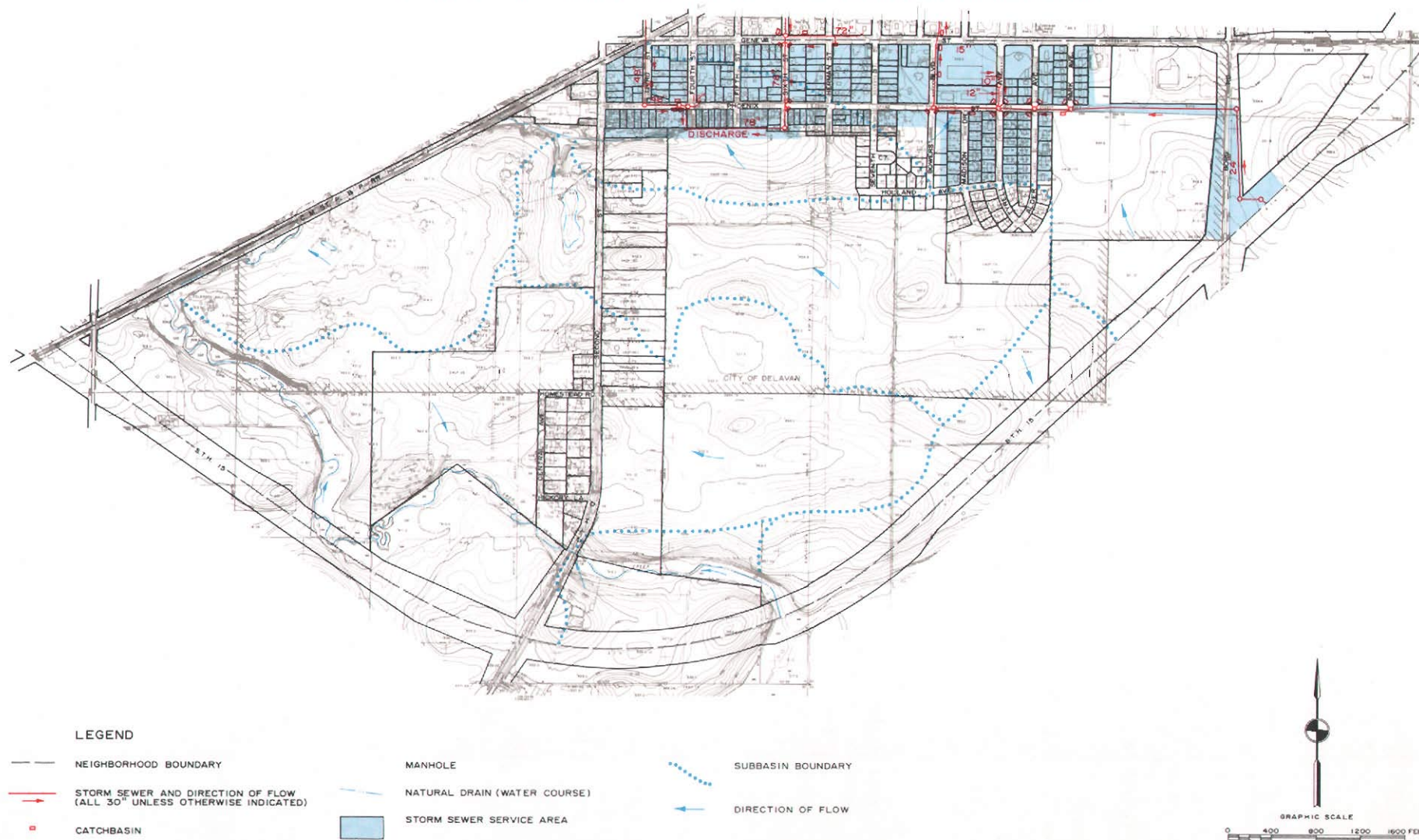
EXISTING PUBLIC WATER SUPPLY SERVICE IN THE DELROCK NEIGHBORHOOD: 1978



Source: SEWRPC.

Map 11

EXISTING STORM SEWER SERVICE IN THE DELROCK NEIGHBORHOOD: 1978



Source: SEWRPC.

Library service is provided by the City of Delavan Public Library. Fire and police protection are provided by the City of Delavan, and general commercial facilities are currently provided by Delavan's central business district as well as by scattered commercial sites through the City. The Delrock Neighborhood is currently served by the Delavan Joint School District No. 1 for elementary education and the Delavan-Darien Union High School District.

STREET AND HIGHWAY FACILITIES

The existing streets and highways within and adjacent to the neighborhood area are shown on Map 3. Select information concerning the existing rights-of-way of those existing streets and highways is set forth in Table 6. Streets and highways, including one-half of the boundary arterial streets and highways, presently account for about 11 percent of the total area of the neighborhood. Arterial streets and highways in the Delrock Neighborhood measure 5.31 miles; collector streets, 1.16 miles; and minor streets, 2.57 miles. A highway located along the southern boundary of the neighborhood, STH 15, is a freeway facility and as such is a major consideration in the neighborhood design.

REAL PROPERTY OWNERSHIP

Three hundred and nine separate parcels of real property exist within the Delrock Neighborhood, ranging in size from 0.18 acre to more than 160

acres. The boundaries of these parcels, together with existing structures and public and private utility and access easements, are shown in their correct location and orientation on Map 12. Easements within the neighborhood provide locations for power, communication, and utility facilities.

LAND USE CONSTRAINTS IN THE DELROCK NEIGHBORHOOD

Several land use problems are evident in the Delrock Neighborhood (see on Map 12). Long, narrow lots on the east side of CTH 0 are wasteful and leave pockets of unusable land in the rear yards that are difficult to further subdivide or replat. These pockets impair the eventual creation of a sound neighborhood street pattern. An excessive number of streets intersect Phoenix Street, creating too many intersections along that street. Direct access is permitted to arterial streets, reducing the traffic-carrying capacity and safety of these streets.

Soils with severe and very severe limitations for urban development exist in the northwestern area of the neighborhood and in the southern area of the neighborhood along Swan Creek. Areas covered by these soils will have to be carefully dealt to avoid the creation of developmental and environmental problems. The areas designated and shaded on Map 12 may be costly to develop due to creek crossings which would be required for access to the southern portions of these areas as noted.

Table 6

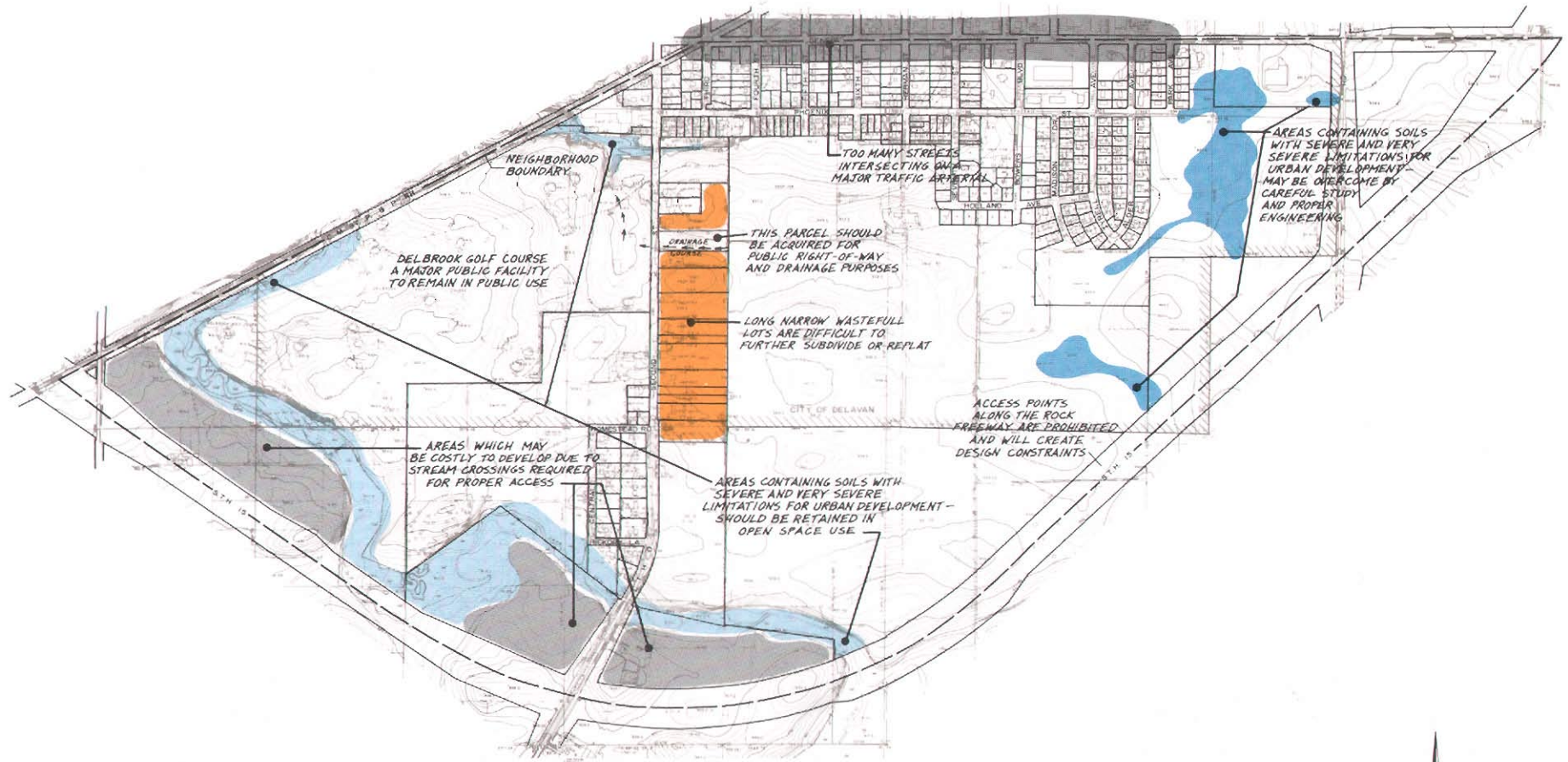
EXISTING STREETS AND HIGHWAYS IN THE DELROCK NEIGHBORHOOD: 1978

Street Classification	Name	Direction	Existing Right-of-Way	Length (miles)
Arterial Streets or Highways	State Trunk Highway 15	East-West	300 feet to 400 feet	2.92
	County Trunk Highway 0 (Second Street)	North-South	66 feet to 250 feet	1.02
	Geneva Street	East-West	66 feet to 200 feet	1.37
	Subtotal			5.31
Collector Streets	Phoenix Street	East-West	66 feet	0.81
	Borg Road	North-South	190 feet to 300 feet	0.35
	Subtotal			1.16
Minor Streets	Adler Avenue	North-South	66 feet	0.32
	Bowers Boulevard	North-South	66 feet	0.28
	Central Avenue	North-South	66 feet	0.18
	Fifth Street	North-South	66 feet	0.10
	Fourth Street	North-South	80 feet	0.10
	Herman Street	North-South	66 feet	0.16
	Hickory Lane	East-West	66 feet	0.08
	Holland Avenue	East-West	66 feet	0.23
	Homestead Road	East-West	66 feet	0.09
	Madison Drive	North-South	66 feet	0.20
	Park Avenue	North-South	66 feet	0.11
	Seventh Street	North-South	66 feet	0.26
	Sixth Street	North-South	66 feet	0.15
	Third Street	North-South	80 feet	0.10
	Tyrell Avenue	North-South	66 feet	0.21
	Subtotal			2.57
Total				9.04

Source: SEWRPC.

Map 12

SUMMARY OF CONSTRAINTS AFFECTING NEIGHBORHOOD DESIGN



Source: SEWRPC.

ALTERNATIVE PLANS FOR THE DELROCK NEIGHBORHOOD

INTRODUCTION

In accordance with general community development objectives and the neighborhood unit design concepts previously outlined herein, a series of neighborhood development plans were prepared for the Delrock Neighborhood. Alternative plans for the neighborhood are shown on Maps 13, 14, 15, and 16, and the recommended plan is shown in graphic form on map 17. The plans were prepared at a scale of 1" = 200' using topographic maps having a vertical contour interval of two feet, to which pertinent cadastral data furnished by the City of Delavan were added. All of the basic data pertinent to good land subdivision design, including data on soil characteristics, topography and drainage patterns, real property boundaries, existing land uses, and utilities, were carefully considered in the designs. In addition, certain design criteria described below were used in the design process.

DESIGN CRITERIA

The following design criteria were used as guides in the design of the Delrock Neighborhood plans. These criteria relate to the layout and design of streets, blocks, lots, easements, and storm water drainage facilities.

Streets

Limitation of Access to Arterial Streets: Whenever proposed residential land uses abut an arterial street or highway, the character of the residential uses and the capacity and safety of the arterial facility should be protected by limiting access from the abutting land uses, and by separating through and local traffic, where possible, by reversed frontage. In addition, a planting screen should be provided in a nonaccess reservation along the rear property line.

Street Cross Sections: Table 7 outlines the design criteria for four-lane arterial streets, two-lane arterial streets, collector streets, minor streets, cul-de-sacs, and pedestrian ways. The respective cross-sections are shown graphically in Appendix A.

Street Grades: Unless necessitated by exceptional topography, the maximum grade of any street should not exceed the following: arterial

streets, 6 percent; collector streets, 8 percent; minor streets, alleys, and frontage streets, 12 percent; and pedestrian ways, 12 percent unless steps of acceptable design are provided. In addition, the grade of any street should in no case exceed 12 percent or be less than 0.5 percent. Street grades should be established so as to avoid excessive grading, the promiscuous removal of ground cover and tree growth, and unnecessary leveling of the topography.

Street Intersections: Streets should intersect each other at as near to right angles as topography and other limiting factors of good design permit. In addition, the number of streets converging at one intersection should be held to a minimum, preferably to not more than two streets at one intersection; the number of intersections along arterial streets and highways should be held to a minimum, and the distance between such intersections should generally not be less than 1,200 feet; and property lines at street intersections should be rounded with a minimum radius of 15 feet or should be cut off by a straight line through the joints of tangency of an arc having a radius of 15 feet.

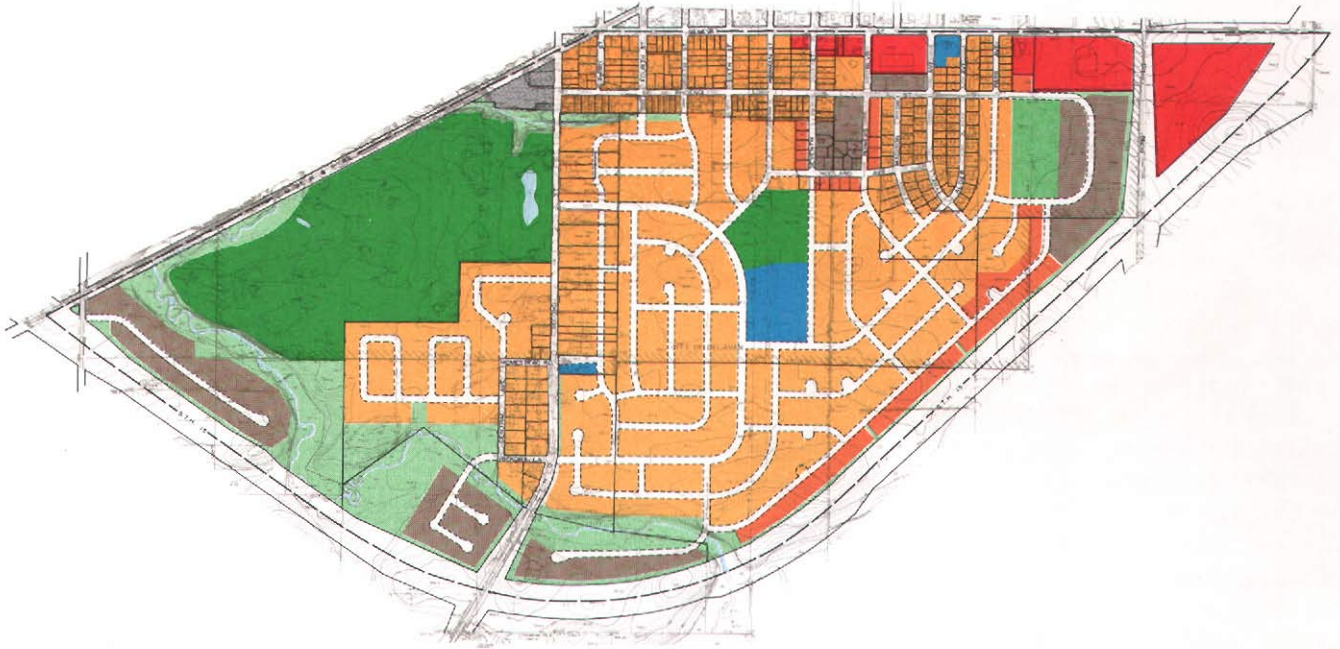
Street Alignment: In addition, minor and collector streets should not necessarily continue across arterial streets. If the distance between the centerline intersections of any street and any other intersecting street is less than 250 feet measured along the centerline of the intersecting streets, then the street location should be adjusted so that the distance is increased or the adjoinment across the intersecting street is continuous, thus avoiding a jog in the flow of traffic.

Half Streets: The platting of half streets should be avoided. Half streets put an unrealistic reliance on the chance that adjacent property owners will develop their adjustment properties at the same time. If half streets are allowed and then improved, their narrow width may result in street maintenance as well as traffic circulation problems.

Cul-de-Sac Streets: Cul-de-sacs which are designed to have one end permanently closed should generally not exceed 600 feet in length. Such cul-de-sac streets should terminate in a circular turnaround having a design as described in Table 7.

Map 13

ALTERNATIVE PRECISE NEIGHBORHOOD UNIT DEVELOPMENT PLAN A FOR THE DELROCK NEIGHBORHOOD



Map 14

ALTERNATIVE PRECISE NEIGHBORHOOD UNIT DEVELOPMENT PLAN B FOR THE DELROCK NEIGHBORHOOD



LEGEND

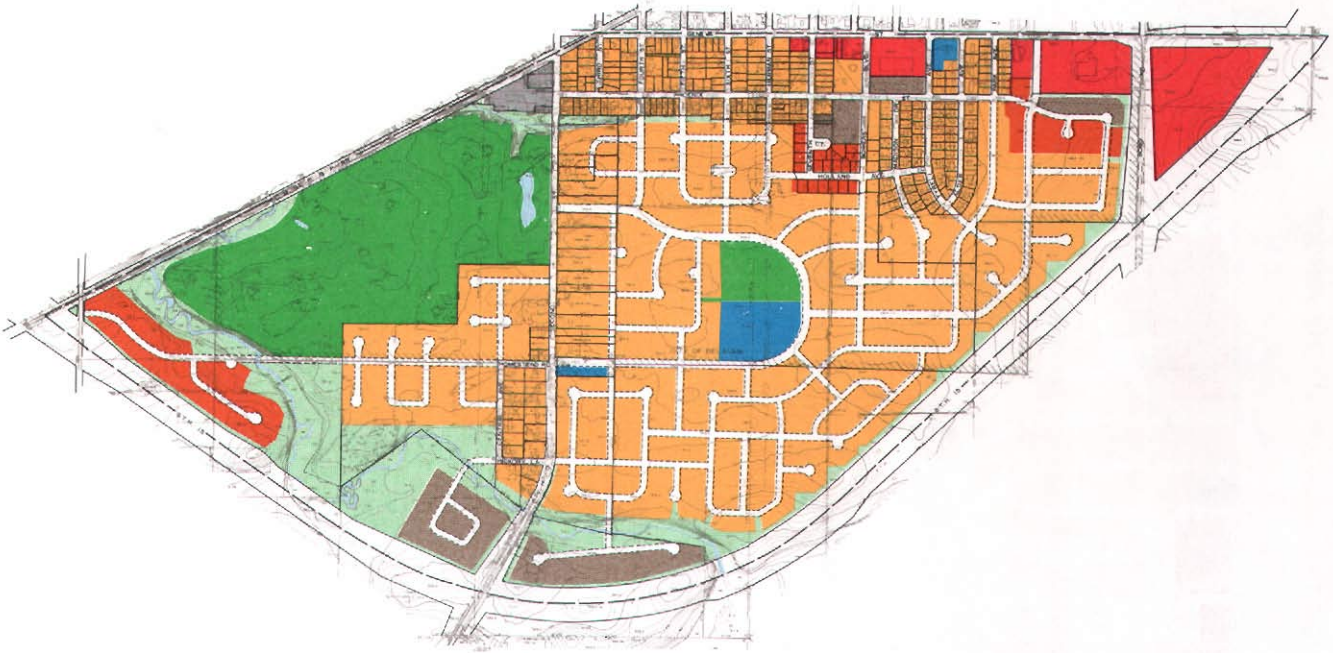
— NEIGHBORHOOD BOUNDARY	GOVERNMENTAL AND INSTITUTIONAL	LAKES AND STREAMS
— PROPERTY BOUNDARY LINE	PARK AND OPEN SPACE	EXISTING TWO-LANE ARTERIAL STREETS AND HIGHWAYS (80' R.O.W. WIDTH)
SINGLE-FAMILY RESIDENTIAL	PRIVATE OPEN SPACE	EXISTING NEIGHBORHOOD COLLECTOR STREETS (80' R.O.W. WIDTH)
TWO-FAMILY RESIDENTIAL	PRIMARY ENVIRONMENTAL CORRIDOR	EXISTING MINOR LAND ACCESS STREETS (66' R.O.W. WIDTH)
MULTIPLE-FAMILY RESIDENTIAL	INDUSTRIAL	
COMMERCIAL		



Source: SEWRPC.

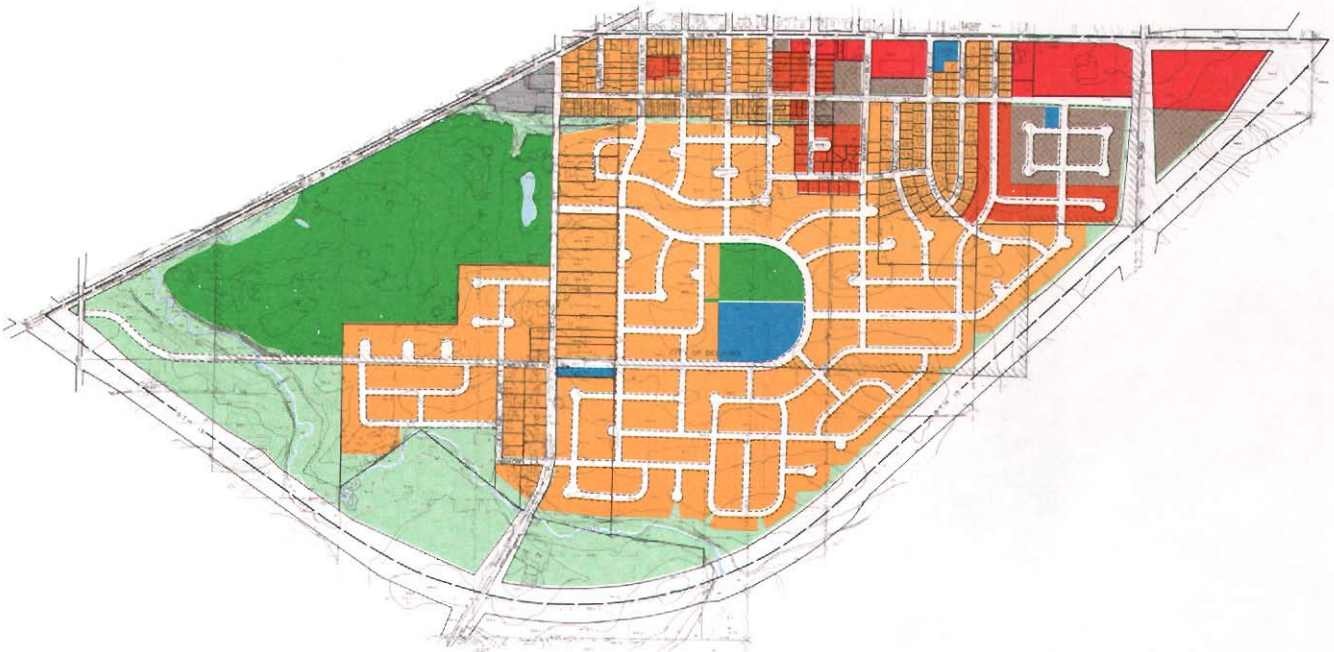
Map 15

ALTERNATIVE PRECISE NEIGHBORHOOD UNIT DEVELOPMENT PLAN C FOR THE DELROCK NEIGHBORHOOD



Map 16

ALTERNATIVE PRECISE NEIGHBORHOOD UNIT DEVELOPMENT PLAN D FOR THE DELROCK NEIGHBORHOOD



LEGEND

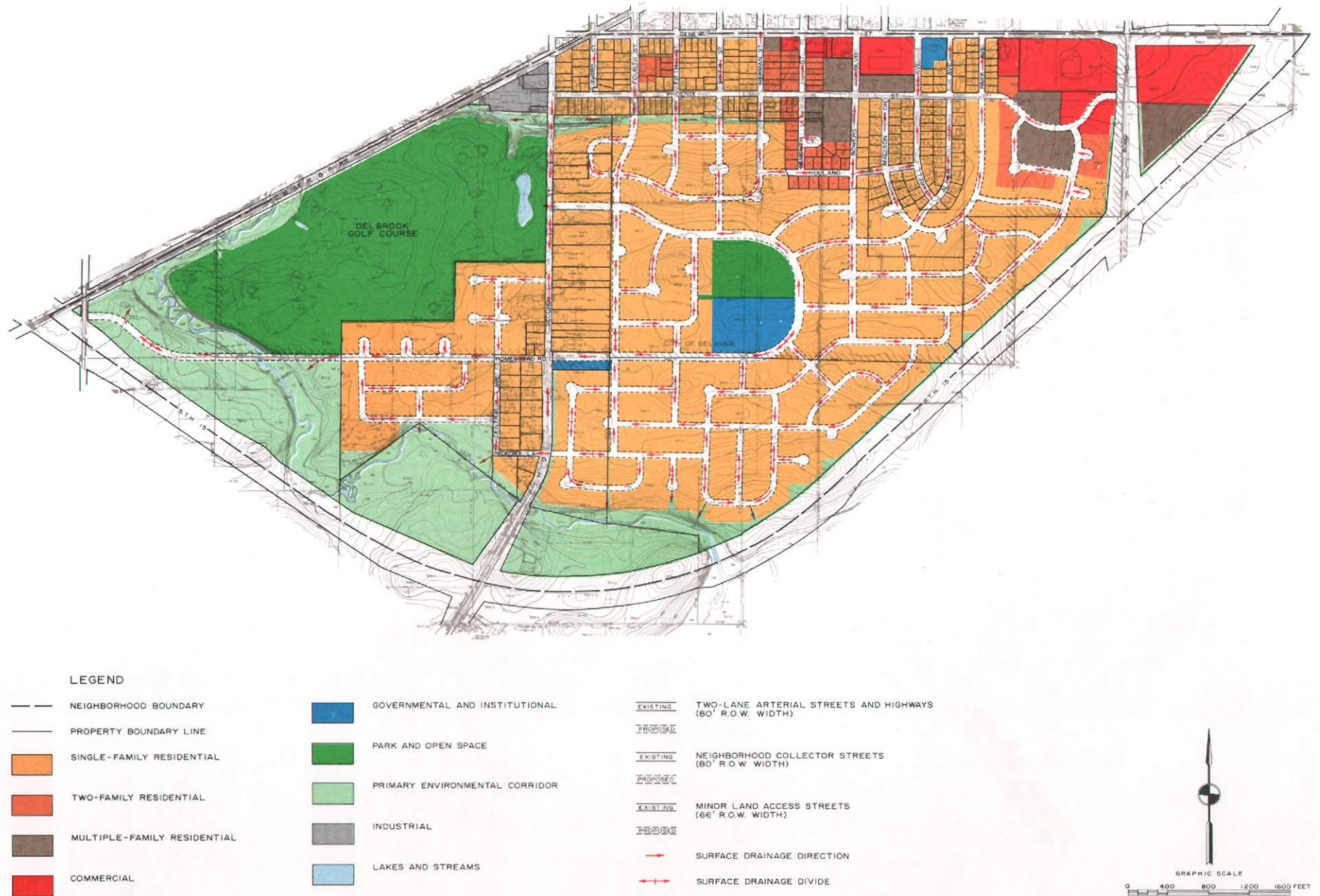
	NEIGHBORHOOD BOUNDARY		GOVERNMENTAL AND INSTITUTIONAL		TWO-LANE ARTERIAL STREETS AND HIGHWAYS (80' R.O.W. WIDTH)
	PROPERTY BOUNDARY LINE		PARK AND OPEN SPACE		NEIGHBORHOOD COLLECTOR STREETS (60' R.O.W. WIDTH)
	SINGLE-FAMILY RESIDENTIAL		PRIMARY ENVIRONMENTAL CORRIDOR		MINOR LAND ACCESS STREETS (66' R.O.W. WIDTH)
	TWO-FAMILY RESIDENTIAL		INDUSTRIAL		
	MULTIPLE-FAMILY RESIDENTIAL		LAKE AND STREAMS		
	COMMERCIAL				



Source: SEWRPC.

Map 17

RECOMMENDED PRECISE NEIGHBORHOOD UNIT DEVELOPMENT PLAN FOR THE DELROCK NEIGHBORHOOD



Source: SEWRPC.

Table 7

STREET DESIGN CRITERIA FOR THE DELROCK NEIGHBORHOOD

Type of Street	Minimum Right-of-Way to be Dedicated	Minimum Dimensions
Arterial Streets (four lane)	130 feet	Dual 36-foot pavement (face of curb to face of curb) 26-foot median 10-foot tree banks (curb lawn) 5-foot sidewalks 1-foot outside sidewalks
Arterial Streets (two lane)	80 feet	48-foot pavement (face of curb to face of curb) 10-foot tree banks (curb lawn) 5-foot sidewalks 1-foot outside sidewalks
Collector Streets	80 feet	48-foot pavement (face of curb to face of curb) 10-foot tree banks (curb lawn) 5-foot sidewalks 1-foot outside sidewalks
Minor Streets	66 feet	36-foot pavement (face of curb to face of curb) 9-foot tree banks (curb lawn) 5-foot sidewalks 1-foot outside sidewalks
Cul-de-sac (turnaround)	75-foot radius	56-foot outside face of curb radius 34-foot inside pavement radius 13-foot tree banks (curb lawn) 5-foot sidewalks (if required) 1-foot outside sidewalks
Pedestrian Ways	20-foot average	To be determined by the City of Delavan on a site specific basis

Source: SEWRPC.

Blocks

The widths, lengths, and shapes of blocks should be suited to the planned use of the land; zoning requirements; the need for convenient access, control, and safety of street traffic; and the limitations of and opportunities provided by topography.

Length: Blocks in residential areas should not, as a rule, be less than 600 feet nor more than 1,200 feet in length unless otherwise dictated by exceptional topography or other limiting factors of good design.

Pedestrian Ways: Pedestrian ways of not less than 20 feet in width may be required near the center and entirely across any block over 900 feet in length to provide adequate pedestrian circulation or access to schools, parks, shopping centers, churches, or transportation facilities.

Width: Blocks should be wide enough to provide for two tiers of lots of appropriate depth except where required to separate residential development from through traffic. Width of lots or parcels

reserved or designated for commercial or industrial use shall be adequate to provide for offstreet service and parking required by the use contemplated and to meet the area zoning restrictions for such use.

Utilities: Telephone and electric power lines should, where practical, be placed on mid-block easements of not less than 20 feet in width centered on the property line and, where possible, along rear lot lines for underground construction.

Lots

The size, shape, and orientation of lots shall be appropriate for the location of the subdivision and for the type of development and use contemplated. The lots should be designed to provide an aesthetically pleasing building site and a proper architectural setting for the building contemplated.

Side Lots: Side lot lines should be at right angles to straight street lines or radial to curved street lines on which the lots face. Lot lines should follow municipal boundary lines rather than cross them.

Double Frontage: Double frontage or "through" lots should be prohibited except where necessary to provide separation of residential development from arterial traffic or to overcome specific disadvantages or topography and orientation.

Access: Every lot should front or abut a public street for a distance of at least 40 feet.

Lot Size: Area and dimensions of all lots should conform to the requirements of the City of Delavan Zoning Code for subdivisions within the neighborhood.

Lot Depth: Excessive depth of lots in relation to width should be avoided and a proportion of two to one should be considered a maximum depth to width ratio. Depth of lots or parcels designated for commercial or industrial use should be adequate to provide for offstreet service and parking required by the use contemplated.

Lot Width: Lots within the interior of a block should have a minimum average width required in the proposed zoning districts for the City of Delavan contained herein as Table 15.

Corner Lots: Corner lots should have an additional width of 10 feet to permit adequate building setbacks from side streets.

Areas of Vegetation

Every effort should be made to protect and retain all existing trees, shrubbery, vines, and grasses not actually lying in public roadways, drainage-ways, paths, and trails. Trees should be protected and preserved during construction in accordance with sound conservation practices, including the preservation of trees by use of wells or islands or retaining walls whenever abutting grades are altered.

Cutting and Clearing: Tree cutting and shrubbery clearing should not exceed 30 percent of the lot or tract and should be conducted so as to prevent erosion and sedimentation and preserve and improve scenic qualities.

Paths: Paths and trails in wooded and wetland areas should not exceed 10 feet in width unless otherwise approved by the City of Delavan and should be designed and constructed so as to result in the least removal and disruption of trees and shrubs and the minimum impairment of natural beauty.

Street Trees: At least one street tree of an approved species and of at least six feet in height should be planted for each 50 feet of frontage on all proposed dedicated streets.

Easements

Utility easements of widths adequate for the intended purpose but not less than 10 feet on each side of all rear lot lines and on side lot lines or across lots may be required by the City of Delavan where necessary or advisable for electric power and communication wires and conduits; storm and sanitary sewers; and gas, water, and other utility lines. Where a subdivision is traversed by a watercourse, drainageway channel, or stream, an adequate drainageway or easement should be provided as may be required by the City Engineer.

Storm Water Drainage and Erosion/Sedimentation Control

Storm water drainage facilities should be adequate to serve the subdivision and may include curbs and gutters, catch basins and inlets, storm sewers, road ditches, culverts, open channels, water retention structures, and settling basins. The facilities should be of adequate size and grade to hydraulically accommodate the maximum potential volumes of flow through and from the subdivision and shall be so designed as to prevent and control soil erosion and sedimentation and to present no hazards to life or property.

Where feasible, storm water drainage should be accommodated in landscaped open channels of adequate size and grade to hydraulically accommodate maximum potential volumes of flow. These design details are subject to review by the City Engineer.

Earth-moving activities such as grading, topsoil removal, mineral extraction, road cutting, waterway construction or enlargement, excavation, channel clearing, ditching, drain tile laying, dredging, and lagooning should be so conducted as to prevent erosion and sedimentation and to least disturb the natural fauna, flora, watercourse, water regime, and topography. Cut and filled lands outside of street right-of-way should be graded to a maximum slope of 25 percent or to the soils' angle of repose.

The subdivider should plant those grasses, trees, and vines--the species and size of which is to be determined by the City--necessary to prevent soil erosion and sedimentation. The City of Delavan may require the subdivider to provide or install certain protection and rehabilitation measures, such as fencing, slopes, seeding, trees, shrubs, riprap, wells, revetments, jetties, clearing, dredging, snagging, drop structures, brush mats, willow poles, and grade stabilization structures.

THE ALTERNATIVE PLANS

Alternative Plan A

Alternative neighborhood unit development plan A is shown on Map 13. The plan represents an initial design presented to the City of Delavan Plan Commission. The plan shows an elongated area of two-family dwellings contiguous to STH 15 in the southeastern portion of the neighborhood which acts as a transitional area between the highway and the remainder of the neighborhood. A large private open space area proposed to be located in the northeast corner of the neighborhood provides a transition area between multiple-family residential land uses and the surrounding lower-intensity land uses. Commercial land uses are proposed in the northeastern portion of the neighborhood and along Geneva Street, where such land uses already exist. A neighborhood park and school site are proposed to be located in the central area of the neighborhood. A drainageway and open space area is provided through the central portion of the neighborhood. Upon review of this initial design, the City Plan

Commission requested that alternative designs be prepared--in particular, an alternative plan proposing industrial uses along STH 15.

Alternative Plan B

Alternative Plan B, as shown on Map 14, includes a proposed industrial area in the southern portion of the neighborhood along STH 15. In order to minimize the conflict between this area of industrial use and adjacent residential areas, the plan proposes an open space buffer area between these generally incompatible land uses. As in Alternative Plan A, commercial land uses are proposed in the northeastern portion of the neighborhood and along Geneva Street, where such land uses already exist. A large private open space area located in the northeast corner of the neighborhood provides a transitional open space area between multiple-family land uses and the surrounding two-family dwelling uses. Two-family dwelling land uses are also shown contiguous to the buffer strip separating the industrial land uses along STH 15 from residential uses in the southeastern portion of the neighborhood. As in Alternative Plan A, a neighborhood park and school site are proposed to be centrally located in the neighborhood. Multiple-family residential uses are proposed at the northwest corner of the intersection of CTH 0 and STH 15, in the northeast corner of the neighborhood, and in the north-central portion of the neighborhood. Consideration of this alternative led to its rejection by the City Plan Commission.

Alternative Plan C

Alternative Plan C is shown on Map 15. Commercial land uses are proposed in the northeastern portion of the neighborhood and along Geneva Street, where such land uses already exist. The neighborhood park and school sites have been centrally located at the intersection of two collector streets. Single-family residential land uses are shown along STH 15 in the southeastern portion of the neighborhood, with an open space buffer zone between these uses and the highway. Multiple-family residential uses are proposed at the northeast and northwest corners of the intersection of CTH 0 and STH 15, in the northeast corner of the neighborhood, and in the north-central portion of the neighborhood. Two-family residential land uses are proposed in the southwest corner of the neighborhood along STH 15, in the northeast corner of the neighborhood, and in the north-central portion of the neighborhood. An elongated drainageway and open space area is

proposed in the southern portion of the neighborhood. This area would extend in an east-west direction and act as a buffer between the more intensive multiple-family development and the less intensive single-family residential areas. Upon review of this alternative, the City Plan Commission concluded that a more extensive buffer area should be provided between STH 15 and the proposed single-family residential area and that the multiple-family and two-family residential uses proposed on the south side of the neighborhood contiguous to STH 15 were inappropriate. The City Plan Commission also determined that both Geneva Street and Phoenix Street should function as arterials.

Alternative Plan D

Map 16 shows Alternative Plan D. A large drainage-way and open space area is proposed in the southern portion of the neighborhood contiguous to STH 15 and adjoining single-family residential land uses. Commercial land uses are proposed in the northeastern portion of the neighborhood along Geneva Street. Multiple-family residential land uses are proposed in the northeast corner of the neighborhood, along with adjoining two-family residential land uses, and in the north-central portion of the neighborhood, also along with adjoining two-family residential uses. The neighborhood park and school sites are proposed to be centrally located at the intersection of two collector streets. Also, a major drainageway has been provided in the northern portion of the neighborhood running in an east-west direction. The industrial uses proposed in the northwest portion of the neighborhood reflect existing land uses.

Introduction to the Recommended Plan

The four alternative neighborhood plans, as presented above, represent stages of the design process leading to a recommended neighborhood plan. The recommended plan incorporates and refines the best features of the alternative plans and reflects opinion gathered at informational meetings held in the community. The recommended neighborhood unit development plan for the Delrock Neighborhood is presented on Map 17. The recommended plan would provide housing for about 1,700 families, or for a total population of about 5,300 persons in single-family, two-family, and multiple-family dwelling units.

Land Use Description: An elementary school is proposed to serve the neighborhood. The school is proposed to be centrally located on a site bounded on one side by a collector street. The

of approximately 10 acres, bounded on one side by a collector street. The proposed school site is contiguous to a proposed neighborhood park. These facilities would be within a maximum walking distance of one mile from all parts of the neighborhood.

A large area in the southern portion of the Delrock Neighborhood is proposed to be retained in natural open space uses. This area is located along Swan Creek and adjacent to STH 15. Pedestrian access from the adjacent single-family residential areas to these open space uses is provided. Portions of the proposed open space area are flood-prone and covered by soils which have severe or very severe limitations for urban development.

Neighborhood commercial facilities are proposed to be located in the northeastern portion of the Delrock Neighborhood along Geneva Street. Multiple-family uses are to be located south of and contiguous to the commercial areas as shown on Map 17. Two-family uses are proposed to be located in an area along 7th Street between Holland Avenue and Geneva Street; an area along Holland Avenue between 7th Street and Bowers Boulevard; in an area north of Phoenix Street bounded by 5th Street on the east and 4th Street on the west; and in an area at the northeast corner of the neighborhood contiguous to proposed multiple-family residential uses.

The recommended plan includes a system of drainageways to facilitate surface water drainage without the construction of large piped storm sewers. The rights-of-way for these drainageways are intended to be of sufficient width to allow for channel deepening and widening while maintaining gentle side slopes and providing ample opportunities for landscape planting. In certain locations these drainageways could also be used as locations for pedestrian and bicycle ways. A large drainageway is also proposed in the northern portion of the Delrock Neighborhood channeling storm water in an east-west direction and to a natural outfall outside of the neighborhood boundary.

The existing 1975 land uses and the proposed land uses as shown on Map 17 are compared in Table 8. The number of acres devoted to each land use category and the proportion which comprises each type of land use in the neighborhood are given. Residential land use in the Delrock Neighborhood represents the single largest proposed land use category, comprising about 52 percent of the total area of the neighborhood when fully developed.

Table 8

EXISTING AND PROPOSED LAND USE IN THE DELROCK NEIGHBORHOOD

Land Use Category	Existing Land Use—1975		Plan Increment		Planned Ultimate Land Use	
	Acres	Percent of Total	Acres	Percent Increase	Acres	Percent of Total
Residential						
Single-Family	91.2	9.6	328.8	34.8	420.0	44.4
Two-Family	1.1	0.1	22.1	2.3	23.2	2.4
Multiple-Family	3.9	0.4	29.1	3.9	33.0	3.5
Subtotal	96.2	10.1	380.0	40.2	476.2	50.3
Commercial						
Neighborhood						
Retail and Service	—	—	20.5	2.2	20.5	2.2
Community						
Retail and Service	15.0	1.6	-15.0	-1.6	—	—
Subtotal	15.0	1.6	5.5	0.6	20.5	2.2
Industrial	4.6	0.5	—	—	4.6	0.5
Governmental and Institutional						
Public	—	—	9.9	1.0	9.9	1.0
Private	7.2	0.8	-5.6	-0.6	1.6	0.2
Subtotal	7.2	0.8	4.3	0.4	11.5	1.2
Park and Recreational						
Neighborhood Parks	—	—	10.3	1.1	10.3	1.1
Community Parks	—	—	—	—	—	—
Other Recreational	118.7	12.5	—	—	118.2	12.5
Subtotal	118.7	12.5	10.3	1.1	128.5	13.6
Streets and Other Public Ways						
Arterial Streets	68.0	7.2	—	—	68.0	7.2
Collector Streets	10.4	1.1	22.6	2.4	33.0	3.5
Minor Land Access Streets	22.0	2.3	83.3	8.8	105.3	11.1
Railroad Right-of-Way	6.4	0.7	—	—	6.4	0.7
Drainageway and Walkways	—	—	70.7	7.5	70.7	7.5
Subtotal	106.8	11.3	176.6	18.7	283.4	30.0
Agricultural, Open Lands, and Unused Lands	597.5	63.2	-576.6	-61.0	20.9	2.2
Total	946.0	100.0	—	—	946.0	100.0

Source: SEWRPC.

Circulation

The proposed street system for the neighborhood is organized on a functional basis and consists of arterial, collector, and land access streets. Arterial streets are arranged so as to facilitate ready access from the neighborhood to centers of employment, governmental activity, shopping and services, and recreation, both within and beyond the boundaries of the community. They are properly integrated with and related to the existing and proposed regional system of major streets and highways and are continuous and in alignment with existing or planned arterial streets and highways with which they are to connect. Five arterial streets or highways are shown on the recommended plan for the Delrock Neighborhood--the STH 15 freeway along the southern boundary of the neighborhood; existing CTH 0 in the central part of the neighborhood extending in a north-south direction; existing Borg Road in the eastern portion of the neighborhood and also extending in a north-south direction; and existing Geneva Street along the northern boundary of the neighborhood. In accordance with City Plan Commission direction, Phoenix Street is also proposed to function as a neighborhood arterial east of 2nd Street. A total of 6.78 miles of arterial streets or highways are contained on the recommended plan.

In order to promote traffic safety and protect the capacity of the arterial street system, the plan proposes to limit direct access of building sites to arterial streets by backing lots against the arterials. The depth of the lots backed against the arterials has been increased over the generally prevailing lot depth within the neighborhood unit to provide room for a planting strip to buffer the residential uses from the arterial streets.

Collector streets are arranged so as to provide ready collection and distribution of traffic from residential areas and conveyance of this traffic to and from the arterial street and highway system and are properly related to special traffic generators such as schools, churches, and shopping centers and to other proposed concentrations of population or activities and to the major streets to which they connect. A collector street is proposed to service the central portion of the neighborhood generally extending in an east-west direction and to loop around the proposed elementary school/neighborhood park site. In addition, existing 6th Street is proposed to serve a collector street, as shown on Map 17.

Under the recommended plan, a total of 3.09 miles of collector streets would serve the Delrock Neighborhood.

The recommended plan proposes the eventual development of 11.21 miles of land access streets, an increase of 8.64 miles over the mileage of existing land access streets in the neighborhood. The proposed land access street network is designed and located to achieve the most efficient use of land; discourage use by through traffic; minimize street area; provide an aesthetic setting for residential development; facilitate the provision of efficient storm water drainage, sewerage, and public water supply facilities; and complement the natural terrain, thereby minimizing the need for grading during the development process. The street locations are based upon consideration of a number of factors, including soil characteristics, topography, property boundaries, a hierarchy within the total street system, land use, the principles of neighborhood planning, and the design criteria presented herein.

Selected data on the proposed street system for the Delrock Neighborhood are set forth in Table 9, which indicates the classification, existing right-of-way, proposed right-of-way, typical cross-section, and length in miles of all streets proposed in the recommended plan.

Relationship of Population Growth Trends and Plan

Tables 10 through 13 summarize pertinent data on total population, school age population, and population density within the Delrock Neighborhood unit for the base year 1970 and for ultimate development in accordance with the recommended plan. The number of existing dwelling units in the Delrock Neighborhood in 1970 was 278, and the resident population was about 900 persons. Upon ultimate development of the neighborhood in accordance with the plan, the number of dwelling units would be 1,711, and the resident population would approximate 5,300 persons, of which 1,500 could be primary and secondary school age children.

Population forecasts prepared by the Regional Planning Commission for the City of Delavan urban area indicate that by the year 2000 the resident population of that area is likely to approximate 8,700 persons, an increase of only 3,174 persons over the 1970 level of 5,526 persons. These forecasts show that it is highly

Table 9

STREETS AND HIGHWAYS IN THE DELROCK NEIGHBORHOOD: 1978 AND UPON ULTIMATE DEVELOPMENT

Classification	Name	Existing Right-of-Way (feet)	Proposed Right-of-Way (feet)	Typical Cross-Section	Length (miles)
Arterial Streets or Highways	State Highway 15	300 feet to 400 feet	—	—	2.92
	County Trunk Highway 0 (2nd Street)	66 feet to 250 feet	80 feet to 250 feet	Desirable two-lane	1.02
	Geneva Street	66 feet to 200 feet	66 feet to 200 feet	Existing two-lane	1.37
	Borg Road	190 feet to 300 feet	—	Minimum four-lane and Desirable four-lane	0.35
	Phoenix Street	66 feet	66 feet	Existing two-lane	1.12
Subtotal					6.78
Collector Streets	6th Street	66 feet	80 feet	Urban Collector	0.38
	Unnamed Collector Streets	N/A	80 feet	Urban Collector	2.69
Subtotal					3.07
Minor Streets	Adler Avenue	66 feet	66 feet	—	0.61
	Bowers Boulevard	66 feet	66 feet	—	0.57
	Central Avenue	66 feet	66 feet	—	0.34
	5th Street	66 feet	66 feet	—	0.28
	4th Street	80 feet	66 feet	—	0.79
	Herman Street	66 feet	66 feet	—	0.30
	Hickory Lane	66 feet	66 feet	—	0.40
	Holland Avenue	66 feet	66 feet	—	0.28
	Homestead Road	66 feet	66 feet	—	0.09
	Madison Drive	66 feet	66 feet	—	0.20
	Park Avenue	66 feet	66 feet	—	0.44
	7th Street	66 feet	66 feet	—	0.26
	3rd Street	80 feet	66 feet	—	0.10
	Tyrell Avenue	66 feet	66 feet	—	0.21
	Unnamed Minor Streets	None	66 feet	Urban Minor	6.34
Subtotal					11.21
Total					21.06

N/A—Indicates data not applicable.

Source: SEWRPC.

Table 10

**ULTIMATE POPULATION, DEVELOPED ACREAGES, AND
RESIDENTIAL DENSITIES IN THE DELROCK NEIGHBORHOOD**

	1970	Planned Increment	Ultimate Development
Population	890	4,414	5,304
Dwelling Units	278	1,433	1,711
Average Family Size	3.2	3.1	3.1
Developed Residential Land (net acres) . . .	96.2	380	476.2
Residential Density (persons per net acre)	9.2	11.6	11.1

Source: SEWRPC.

Table 11

**ULTIMATE PRIMARY AND SECONDARY SCHOOL AGE POPULATION
BY GRADES, BY SCHOOL TYPE, AND BY AVERAGE DAILY ATTENDANCE**

School Grades	Total Enrollment	Private School Enrollment		Public School Enrollment		Average Daily Public Attendance
		Students	Percent of Total	Students	Percent of Total	
K-6	544	54	10.0	491	90.0	437
7-8	487	73	15.0	414	85.0	414
9-12	471	9	2.0	462	98.0	462
Total	1,504	136	9.1	1,367	90.9	1,314

Source: SEWRPC.

Table 12

**ESTIMATED POPULATION DISTRIBUTION BY AGE GROUP IN THE
DELROCK NEIGHBORHOOD: 1970 AND UPON ULTIMATE DEVELOPMENT**

Age Group	1970 Population		Ultimate Population	
	Persons	Percent of Total	Persons	Percent of Total
Under 5	87	9.8	520	9.8
5	18	2.0	105	2.0
6-9	74	8.3	440	8.3
10-13	82	9.2	488	9.2
14-17	79	8.9	472	8.9
18 and older	550	61.8	3,278	61.8
Total	890	100.0	5,304	100.0

Source: SEWRPC.

Table 13

DISTRIBUTION OF ULTIMATE RESIDENTIAL DEVELOPMENT IN THE DELROCK NEIGHBORHOOD

Dwelling Type	Developed Residential Acres	Dwelling Units	Net Density (dwelling units per acre)	School Age Children per Dwelling Unit	Total School Age Children	Population per Dwelling Unit	Total Population
Single-Family	420.0	995	2.4	1.08	1,071	3.5	3,484
Two-Family	23.2	142	6.1	0.96	137	3.1	443
Multiple-Family	33.0	574	17.3	0.52	296	2.4	1,377
Total	476.2	1,711	3.6	0.88	1,504	3.1	5,304

Source: SEWRPC.

unlikely that the Delrock Neighborhood or any of the five other delineated neighborhoods in the Delavan urban area will be fully developed by the turn of the century. The neighborhood plan presented herein should thus be considered as an "ultimate end stage" plan—a plan intended to be used as a point of departure in making development decisions over the years in order to avoid costly mistakes that could create developmental problems and to guide actual piecemeal development over time into a coordinated and harmonious whole. In this respect, it must be recognized that over long periods of time, socioeconomic and related cultural conditions and, therefore, development standards and practices may change, and such change may dictate changes in the adopted neighborhood plan. Officials must accordingly remain flexible in the use and application of the plan, and the plan itself should be updated on a periodic basis. Future changes in the primary means of transportation may alter the concepts embraced in the preparation of the Delrock Neighborhood plan. Similarly, significant socioeconomic changes could occur which would result in a public desire for housing types and styles different from those now prevalent, thus requiring a change in the plan.

Nevertheless, at present and for the near-term future, the proposed Delrock Neighborhood unit plan, as presented herein, offers a sound guide to the rational physical development of the delineated neighborhood. Proper utilization of the plan by city officials can provide the many benefits, for example:

1. The plan provides a framework in which proposed land uses can be properly related to other existing and probable future land uses in the area, and to transportation, utility, and storm water drainage needs

and facilities. The plan provides for the development of a basic street network able to efficiently and safely move traffic into and out of, as well as within the neighborhood. The proposed street pattern also provides the basic public rights-of-way necessary to efficiently accommodate utilities and storm water drainage.

2. The plan can accommodate a diversity of housing types and styles and can accommodate a wide range of land subdivision proposals.
3. The plan identifies areas containing significant natural resources which should be permanently preserved in essentially open natural uses and which can serve to enhance other land uses in the areas.
4. The plan recognizes soil types and accommodates the associated limitations on development in order to avoid the creation of serious and costly developmental and environmental problems.
5. The plan presents staged proposals for zoning district changes together with an outline, in tabular form, for zoning text changes which can assist in implementing the plan.
6. The plan provides for the identification and preservation of sites for such desirable neighborhood facilities as an elementary school and a neighborhood park.

As already noted, the plan should be applied in a thoughtful, flexible manner, and the City Plan Commission must assume the final responsibility of determining when, where, and how future development is to take place in the neighborhood.

The plan can, however, provide the Plan Commission with a broad view of how individual development proposals may be fit into the neighborhood as a whole without creating problems.

PLAN EVALUATION IN TERMS OF LOT YIELD

One of the factors affecting the cost of improved building sites is the economic efficiency of the land subdivision design; that is, the yield in terms of the number of lots per acre which can be obtained from a particular piece of land. This yield is affected by many factors. Some are direct--lot size, block length, and street width--and some indirect--street pattern, topography, the size and shape of the parcel to be subdivided, and the amount and location of common open space. The effect of these factors on lot yield can be determined only through an analysis of individual sites and completed subdivision designs.

Subdivision Lot Yield Efficiency Factors

The subdividing of land normally includes the creation of one or a series of blocks composed of lots, the size of both depending in part upon local zoning and land subdivision control regulations.

The lot size is primarily determined by zoning regulations in the form of a minimum lot area and a minimum lot width along with a corresponding minimum lot depth. As a part of the Southeastern Wisconsin Regional Planning Commission's study of historic land subdivision within the Region from 1920 through 1969, as documented in SEWRPC Technical Report No. 9, Residential Land Subdivision in Southeastern Wisconsin, theoretical maximum lot yields were developed for a full range of urban lot widths and depths.

Lot Yield Efficiency Analysis

After a subdivision has been designed, the actual yield of lots per gross residential acre can be computed. The lot yield efficiency factor for the design can then be computed by dividing the actual yield by the theoretical maximum yield for the same size lot; the larger this factor, the more efficient the design. The theoretical maximum and actual yields were determined for the lot sizes created in the Delrock Neighborhood design, and the efficiency factor was computed. This factor is compared in Table 14 with historic (1920-1969) design efficiency data.

Table 14

LOT YIELD EFFICIENCY FACTORS FOR THE DELROCK NEIGHBORHOOD

Zoning District	Lot Size (square feet)	Lot Width (feet)	Lot Depth (feet)	Zoned Area in the Neighborhood (gross acres)	Number of Lots	Actual Yield in Lots per Acre	Theoretical Maximum Yield in Lots per Acre	Theoretical Maximum Number of Lots	Efficiency Factor (percent)
Rs-1	16,000	100	160	18.8	19	0.98	2.18	40	47.5 ^a
Rs-2	13,000	80	165	288.0	578	2.00	2.66	766	75.5
Rs-3	10,000	75	135	110.5	272	2.46	3.24	358	76.0
Rs-4	8,000	65	120	50.0	126	2.52	4.15	207	60.9 ^a
Rd-2	10,000	70	140	29.0	71	2.44	3.46	100	71.0 ^a
Total				506.3	1,066	2.11	2.91	1,471	72.5

^aThe Rs-1, Rs-4, and Rd-2 zoned areas in the Delrock Neighborhood consist mostly of existing subdivision plats which cannot practically be redivided.

Source: SEWRPC.

Chapter IV

IMPLEMENTATION OF THE NEIGHBORHOOD PLAN

INTRODUCTION

The design of a neighborhood unit development plan is only the first in a series of public and private actions required for the ultimate development of the neighborhood in accordance with the plan. The major steps necessary to formally adopt and implement the Delrock Neighborhood plan include: public informational meetings and a formal hearing, formal adoption by the City Plan Commission, zoning, official mapping, and subdivision plat review.

PUBLIC INFORMATIONAL MEETINGS AND HEARING

Although the Wisconsin city planning enabling legislation does not require local plan commissions to hold public hearings on proposed plan elements prior to adoption of those elements, it is nevertheless recommended that in order to provide for and promote active citizen participation in the planning process, the City Plan Commission hold one or more public informational meetings and a formal public hearing to acquaint neighborhood residents and landowners with all details of the proposed plan and to solicit public reaction to the plan proposals. The plan should be modified to incorporate any desirable new ideas which may be advanced at the informational meetings and hearing.

In accordance with this recommendation, a combined public informational meeting and hearing was held by the City of Delavan Plan Commission on preliminary designs for the Delrock Neighborhood on May 22, 1978. The hearing was well attended, and the official minutes of the hearing are on file in the City Hall. Certain changes were made in the alternative plans as a result of the information provided by this hearing as described earlier in this report. A summary of proposed ultimate development in the Delrock Neighborhood is presented in Appendix B.

PLAN ADOPTION

It is important to plan implementation that the recommended plan be formally adopted. A suggested resolution to adopt the plan is presented in Appendix C. Upon adoption of the plan by the Plan Commission, the secretary should certify such adoption to the Common Council in accordance

with Wisconsin's city planning enabling act. Upon such adoption, the plan becomes the official guide to the making of development decisions concerning the neighborhood by city officials.

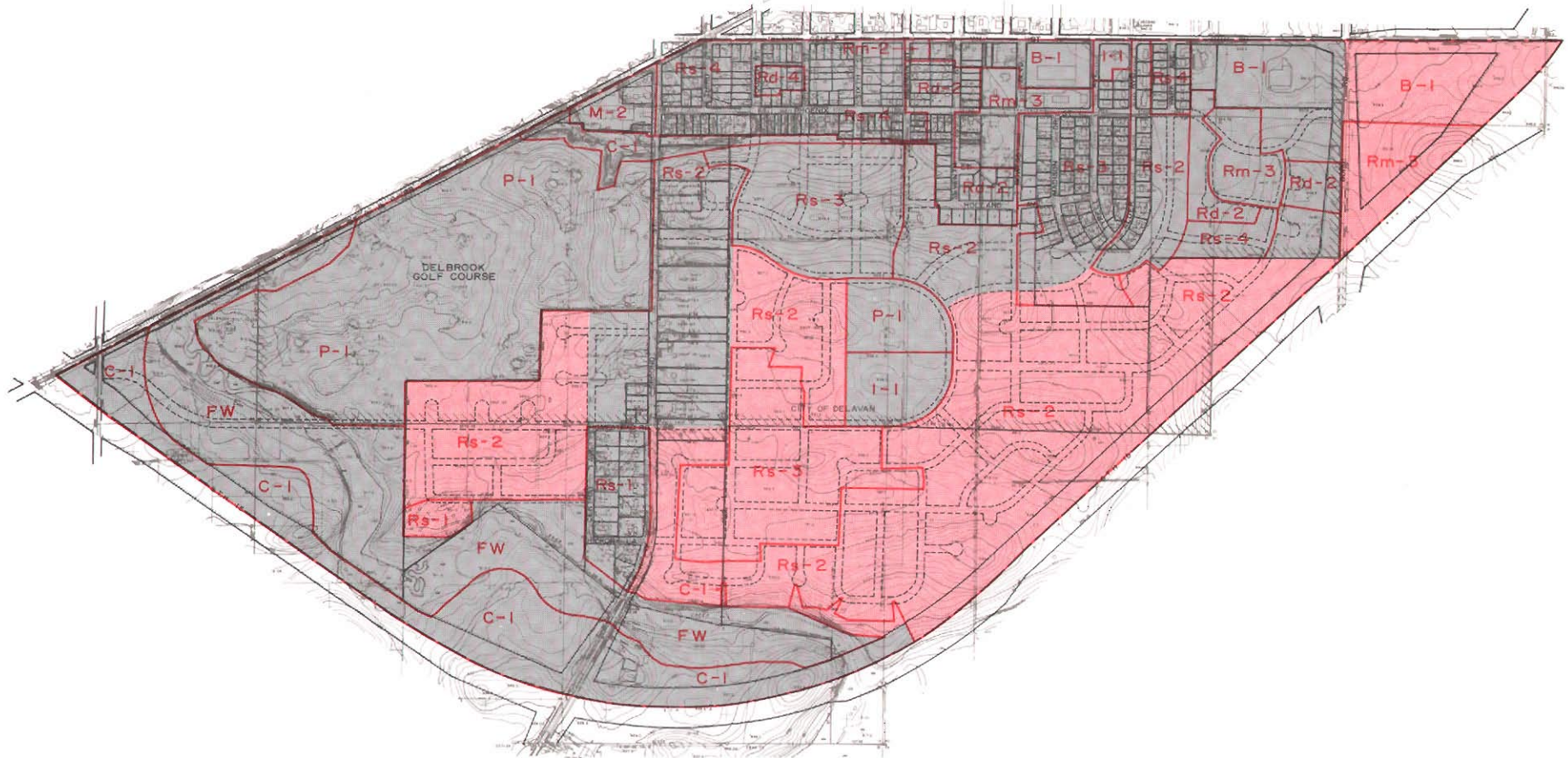
ZONING

Following adoption of the plan by the City Plan Commission and certification to the Common Council, the City Plan Commission should initiate amendments to the city zoning district map and zoning ordinance to bring that map and ordinance into conformance with the proposals advanced in the adopted neighborhood plan as presented herein. Map 18 shows the zoning district boundaries required to implement the plan and sets forth a zoning plan and a staging plan to follow in order to attain the necessary ultimate neighborhood plan implementation. Map 19 shows the initial zoning changes recommended in order to achieve the neighborhood plan. Table 15 provides a summary of the recommended zoning district regulations for each district within the neighborhood. Pursuant to state enabling legislation, the zoning changes recommended by the Plan Commission must be enacted by the Common Council after formal public hearing.

OFFICIAL MAPPING

Following adoption of the neighborhood plan, existing and proposed streets, highways, parks, parkways, and playgrounds shown on the plan should be incorporated into the official map for the City of Delavan. The City Engineer should have an official map sheet covering the neighborhood drafted showing existing and proposed streets, parks, parkways and other public ways. The City Plan Commission and Common Council should act to adopt the map sheet after a public hearing. It should be noted that Wisconsin Statutes specifically provide that the approval of a subdivision plat by the Common Council constitutes an amendment to the Official Map, thus providing flexibility in its administration. A suggested Official Map Ordinance is contained in Appendix D of this report. The Ordinance suggests an official map index to facilitate adoption of the Official Map sheets for those portions of the City for which precise plans have been prepared.

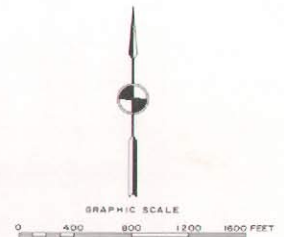
PROPOSED ULTIMATE ZONING MAP FOR THE DELROCK NEIGHBORHOOD



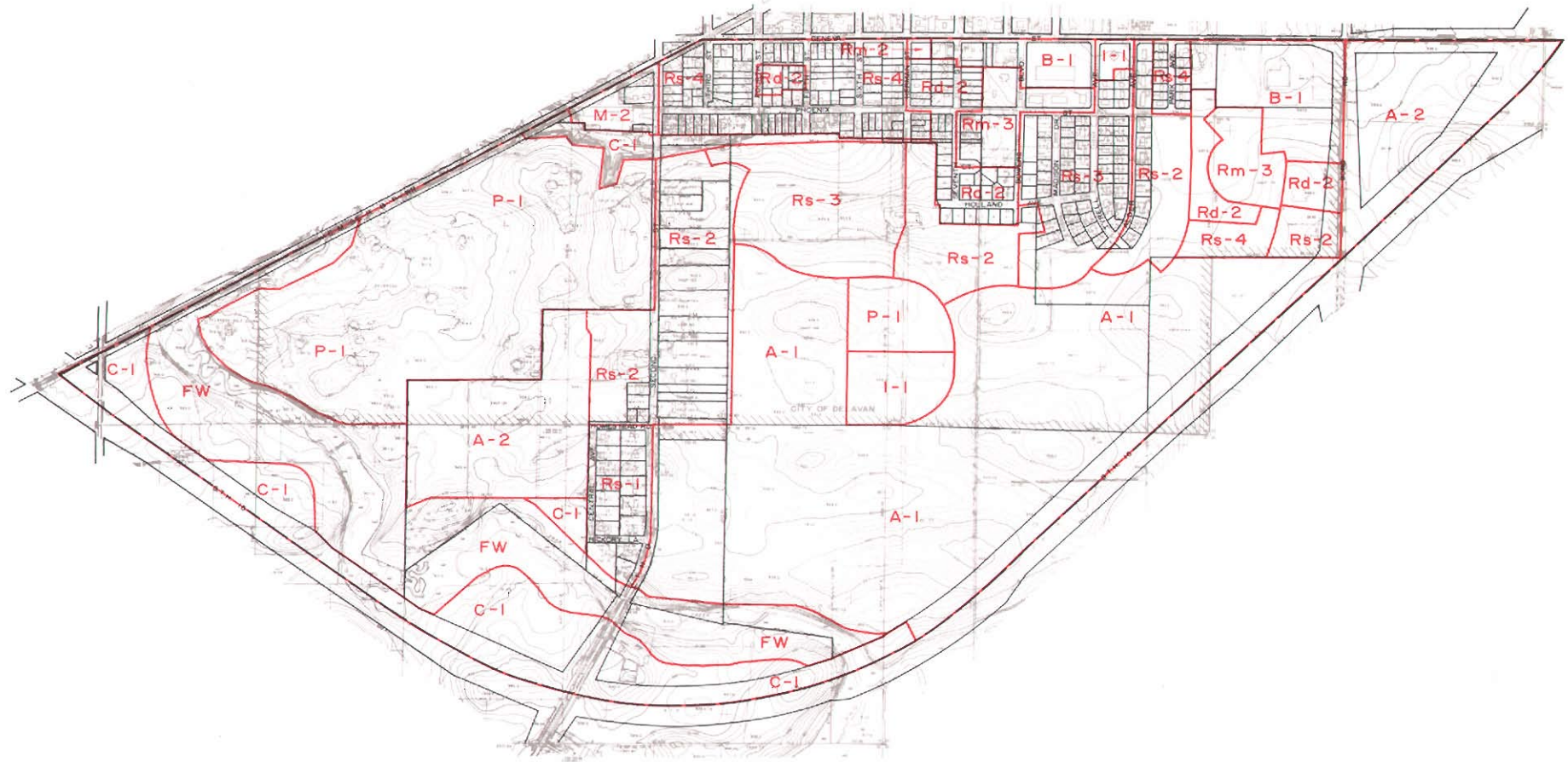
LEGEND

	NEIGHBORHOOD BOUNDARY	Rs-4	SINGLE-FAMILY RESIDENTIAL DISTRICT	B-4	PROFESSIONAL OFFICE DISTRICT	FW	FLOODWAY DISTRICT
	ZONING DISTRICT BOUNDARY	Rs-5	SINGLE-FAMILY RESIDENTIAL DISTRICT	B-5	HIGHWAY BUSINESS DISTRICT	FC	FLOODPLAIN CONSERVANCY DISTRICT
	FIRST STAGE DEVELOPMENT	Rd-1	TWO-FAMILY RESIDENTIAL DISTRICT	M-1	GENERAL BUSINESS AND WAREHOUSING DISTRICT	FFO	FLOODPLAIN FRINGE OVERLAY DISTRICT
	SECOND STAGE DEVELOPMENT	Rd-2	TWO-FAMILY RESIDENTIAL DISTRICT	M-2	LIMITED MANUFACTURING DISTRICT		
A-1	EXCLUSIVE AGRICULTURAL DISTRICT	Rm-1	MULTIPLE-FAMILY RESIDENTIAL DISTRICT	M-3	GENERAL MANUFACTURING DISTRICT		
A-2	AGRICULTURAL/HOLDING DISTRICT	Rm-2	MULTIPLE-FAMILY RESIDENTIAL DISTRICT	I-1	INSTITUTIONAL DISTRICT		
Rs-1	SINGLE-FAMILY RESIDENTIAL DISTRICT	Rm-3	MULTIPLE-FAMILY RESIDENTIAL DISTRICT	P-1	PARK DISTRICT		
Rs-2	SINGLE-FAMILY RESIDENTIAL DISTRICT	B-1	NEIGHBORHOOD BUSINESS DISTRICT	C-1	LOWLAND RESOURCE CONSERVANCY DISTRICT		
Rs-3	SINGLE-FAMILY RESIDENTIAL DISTRICT	B-2	COMMUNITY BUSINESS DISTRICT				
		B-3	CENTRAL BUSINESS DISTRICT				

Source: SEWRPC.



RECOMMENDED INITIAL ZONING MAP FOR THE DELROCK NEIGHBORHOOD



LEGEND

—	NEIGHBORHOOD BOUNDARY	Rd-1	TWO-FAMILY RESIDENTIAL DISTRICT	M-1	GENERAL BUSINESS AND WAREHOUSING DISTRICT
—	ZONING DISTRICT BOUNDARY	Rd-2	TWO-FAMILY RESIDENTIAL DISTRICT	M-2	LIMITED MANUFACTURING DISTRICT
A-1	EXCLUSIVE AGRICULTURAL DISTRICT	Rm-1	MULTIPLE-FAMILY RESIDENTIAL DISTRICT	M-3	GENERAL MANUFACTURING DISTRICT
A-2	AGRICULTURAL / HOLDING DISTRICT	Rm-2	MULTIPLE-FAMILY RESIDENTIAL DISTRICT	I-1	INSTITUTIONAL DISTRICT
Rs-1	SINGLE-FAMILY RESIDENTIAL DISTRICT	Rm-3	MULTIPLE-FAMILY RESIDENTIAL DISTRICT	P-1	PARK DISTRICT
Rs-2	SINGLE-FAMILY RESIDENTIAL DISTRICT	B-1	NEIGHBORHOOD BUSINESS DISTRICT	C-1	LOWLAND RESOURCE CONSERVANCY DISTRICT
Rs-3	SINGLE-FAMILY RESIDENTIAL DISTRICT	B-2	COMMUNITY BUSINESS DISTRICT	FW	FLOODWAY DISTRICT
Rs-4	SINGLE-FAMILY RESIDENTIAL DISTRICT	B-3	CENTRAL BUSINESS DISTRICT	FC	FLOODPLAIN CONSERVANCY DISTRICT
Rs-5	SINGLE-FAMILY RESIDENTIAL DISTRICT	B-4	PROFESSIONAL OFFICE DISTRICT	FFO	FLOODPLAIN FRINGE OVERLAY DISTRICT
		B-5	HIGHWAY BUSINESS DISTRICT		

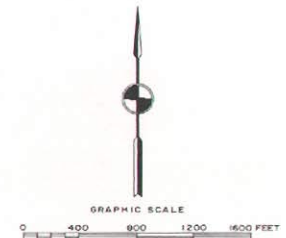


Table 15

A SUMMARY OF SUGGESTED ZONING DISTRICTS FOR THE CITY OF DELAVAN

Zoning District	Permitted Uses		Conditional Uses	Maximum Residential Density (dwelling units per net acre)	Minimum Lot Size			Minimum Yard Requirements			Minimum Building Size			Maximum Building Height (feet)
	Principal	Accessory			Total Area (square feet)	Area per Family (square feet)	Width at Setback (feet)	Front Yard (feet)	Side Yard (feet)	Rear Yard (feet)	Total Area (square feet)	Area per Family (square feet)	First Floor Area (square feet)	
A-1 Exclusive Agricultural District	Crop production, livestock, orchards	Farm dwellings and farm buildings	Fur farms, commercial egg production, airfields, animal clinics	1 unit per 35 acres	35 acres	35 acres	330	50	25	50	800	800	800	60
A-2 Agricultural/Holding District	Crop production, livestock, orchards	Farm dwellings and farm buildings	Fur farms, commercial egg production, airfields, animal clinics	0.2	217,800 (5 acres)	217,800 (5 acres)	300	50	25	50	800	800	800	60
Rs-1 Single-Family Residential District	Single-family dwelling with attached garage	Home occupations	Planned unit developments	2.7	16,000	16,000	100	25	15	25	1,300	1,300	900	35
Rs-2 Single-Family Residential District	Single-family dwelling with attached garage	Home occupations	Planned unit developments	3.3	13,000	13,000	80	25	15	25	1,200	1,200	800	35
Rs-3 Single-Family Residential District	Single-family dwelling	Home occupations, detached garages	Planned unit developments	4.4	10,000	10,000	75	25	10	25	1,000	1,000	800	35
Rs-4 Single-Family Residential District	Single-family dwelling	Home occupations, detached garages	Planned unit developments	5.4	8,000	8,000	65	25	8	25	1,000	1,000	800	35
Rs-5 Single-Family Residential District	Single-family dwelling on lots existing January 1, 1978	Home occupations, detached garages	None	7.3	6,000	6,000	55	25	6	25	800	800	800	35
Rd-1 Two-Family Residential District	Two-family dwellings	Home occupations	Planned unit developments	5.8	15,000	7,500	90	25	15	25	2,000	1,000	1,500	35
Rd-2 Two-Family Residential District	Two-family dwellings	Home occupations	Planned unit developments	8.7	10,000	5,000	70	25	10	25	1,600	800	1,000	35
Rm-1 Multiple-Family Residential District	Multiple-family dwellings not to exceed four units per structure	Home occupations	Planned unit developments	12.4	10,000	Efficiency and one bedroom: 3,500 two bedroom: 5,000 three bedroom or more: 7,500	120	25	15	25	1,500	Efficiency and one bedroom: 500 two bedroom: 750 three bedroom or more: 1,000	1,000	35
Rm-2 Multiple-Family Residential District	Multiple-family dwellings not to exceed eight units per structure	Home occupations	Planned unit developments	14.5	12,000	Efficiency and one bedroom: 3,000 two bedroom: 4,000 three bedroom or more: 6,000	120	35	20	25	2,000	Efficiency: 400 one bedroom: 500 two bedroom: 650 three bedroom or more: 800	1,000	35

Table 15 (continued)

Zoning District	Permitted Uses		Conditional Uses	Maximum Residential Density (dwelling units per net acre)	Minimum Lot Size			Minimum Yard Requirements			Minimum Building Size			Maximum Building Height (feet)
	Principal	Accessory			Total Area (square feet)	Area per Family (square feet)	Width at Setback (feet)	Front Yard (feet)	Side Yard (feet)	Rear Yard (feet)	Total Area (square feet)	Area per Family (square feet)	First Floor Area (square feet)	
Rm-3 Multiple-Family Residential District	Multiple-family dwellings	Home occupations	Planned unit developments and elderly housing	17.4	20,000	Efficiency and one bedroom: 2,500 two bedroom: 3,000	120	35	20	25	3,000	Efficiency: 350 one bedroom: 450 two bedroom: 550	2,000	35
Rm-4 Mobile Home Park/ Subdivision District	Mobile homes on lots in a mobile home subdivision	Home occupations garages, storage sheds	Mobile home parks	6.0	7,200	7,200	60	30	10	25	600	600	—	15
B-1 Neighborhood Business District	Neighborhood shopping centers	Parking and loading areas	None	—	2 acres	—	200	100	40	40	—	—	—	35
B-2 Community Business District	Larger scale shopping centers	Parking and loading areas	None	—	10 acres	—	500	100	40	40	—	—	—	35
B-3 Central Business District	Retail establishments, office buildings	Parking and loading areas, residential units on a nonground floor	Automotive sales and service, radio and TV stations	—	4,800	—	40	—	—	25	—	Efficiency: 350 one bedroom: 450 two bedroom: 550	—	45
B-4 Professional Office District	Professional offices, financial institutions, real estate offices, clinics, studios	Parking and loading areas	Funeral homes	—	10,000	—	80	25	10	25	—	—	—	35
B-5 Highway Business District	Gasoline service stations, motels and hotels, automotive sales and service	Parking and loading areas	Drive-in establishments	—	10,000	—	80	25	10	25	—	—	—	35
M-1 General Wholesale/ Retail and Warehousing District	Gasoline service stations, automotive sales and service, printing services	Parking and loading areas	Lumber yards, warehousing, wholesale/ retail sales, locker plants	—	10,000	—	80	40	10	25	—	—	—	35

Table 15 (continued)

Zoning District	Permitted Uses		Conditional Uses	Maximum Residential Density (dwelling units per net acre)	Minimum Lot Size			Minimum Yard Requirements			Minimum Building Size			Maximum Building Height (feet)
	Principal	Accessory			Total Area (square feet)	Area per Family (square feet)	Width at Setback (feet)	Front Yard (feet)	Side Yard (feet)	Rear Yard (feet)	Total Area (square feet)	Area per Family (square feet)	First Floor Area (square feet)	
M-2 Limited Manufacturing District	Small manufacturers and processors, and warehousing	Parking and loading areas	Outside storage	—	4,800	—	40	—	—	25	—	—	—	45
M-3 General Manufacturing District	Heavy manufacturing	Parking and loading areas	Nuisance industries	—	40,000	—	150	40	25	25	—	—	—	60
I-1 Institutional District	Public office buildings, schools, churches	Parking, loading and related residential quarters	Airports, utilities, cemeteries, hospitals, rest homes, penal institutions	—	10,000	—	80	25	10	25	—	—	—	35
P-1 Park District	Parks, playgrounds and playfields	Parking and storage	Golf courses, campgrounds, marinas	—	—	—	—	40	40	40	—	—	—	35
C-1 Lowland Resource Conservancy District	Open space uses, not including structures	Parking	Golf courses, sportsman's clubs, shooting ranges	—	—	—	—	—	—	—	—	—	—	—
FW Floodway District	Open space uses, not including structures	None	Navigational structures, bridges, utilities, bulkhead lines	—	—	—	—	—	—	—	—	—	—	—
FC Floodplain Conservancy District	Open space uses, not including structures	None	Navigational structures, bridges, utilities, bulkhead lines	—	—	—	—	—	—	—	—	—	—	—
FFO Floodplain Fringe Overlay District	Open space uses, not including structures	None	Filling, structures on fill	— ^a	— ^a	— ^a	— ^a	— ^a	— ^a	— ^a	— ^a	— ^a	— ^a	— ^a

^a As per underlying basic use district requirements.

Source: SEWRPC.

SUBDIVISION PLAT REVIEW

Following adoption of the neighborhood unit plan, the plan should serve as a basis for the preparation of preliminary and final land subdivision plats within the neighborhood. In this respect, the

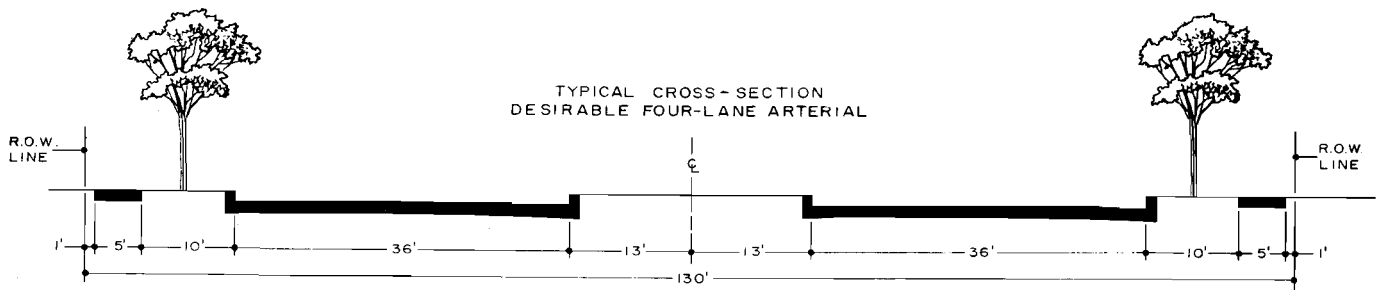
neighborhood plan should be regarded as a point of departure against which to evaluate proposed subdivision plats. Developers should be required to fully justify any proposed departures from the plan, demonstrating that such departures are and improvement to, or proper refinement of, the adopted plan.

APPENDICES

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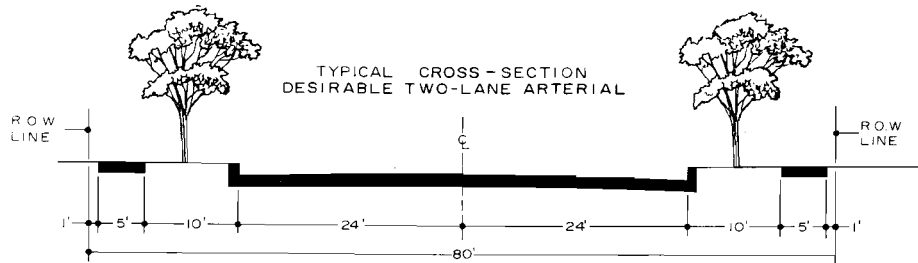
Appendix A

TYPICAL URBAN STREET AND HIGHWAY CROSS-SECTIONS FOR THE DELROCK NEIGHBORHOOD



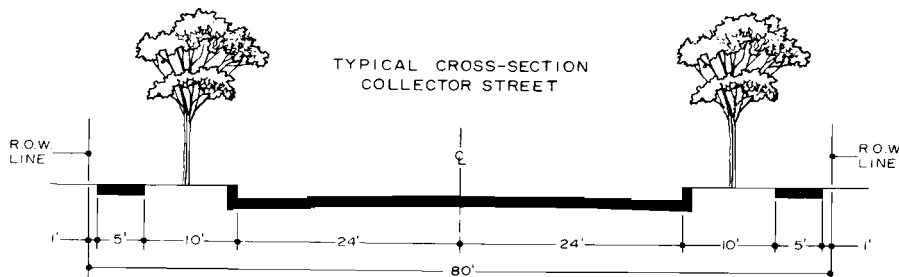
6" GRAVEL BASE
DUAL 36' HIGH TYPE PAVEMENT, 130' R.O.W.
SIDEWALK, STREET LIGHTING
ESTIMATED COST PER MILE:
CONSTRUCTION = \$ 775,000
RESURFACE = \$ 31,800
MAINTENANCE = \$ 6,500 (ANNUAL)

CAPACITY RANGE:
16,400 TO 19,000 VEHICLES PER DAY



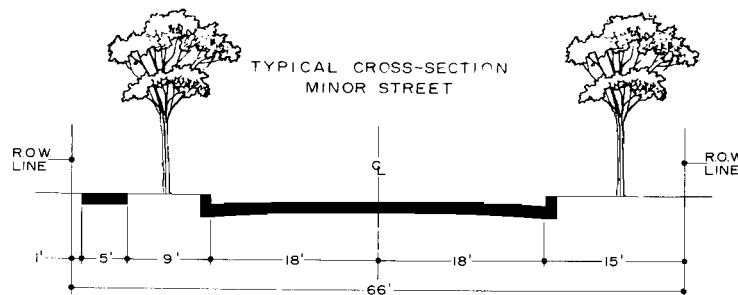
6" GRAVEL BASE
48' HIGH TYPE PAVEMENT, 80' R.O.W.
(ADDITIONAL R.O.W. MAY BE RESERVED IN
UNDEVELOPED AREAS)
SIDEWALK, STREET LIGHTING
ESTIMATED COST PER MILE:
CONSTRUCTION = \$ 390,000
RESURFACE = \$ 21,700
MAINTENANCE = \$ 4,800 (ANNUAL)

CAPACITY RANGE:
10,500 TO 12,000 VEHICLES PER DAY



6" GRAVEL BASE
48' HIGH TYPE PAVEMENT
80' R.O.W.

ESTIMATED COST PER MILE:
CONSTRUCTION = \$ 255,000
RESURFACE = \$ 21,700
MAINTENANCE = \$ 2,600 (ANNUAL)



6" GRAVEL BASE
34' HIGH TYPE PAVEMENT
66' R.O.W.

ESTIMATED COST PER MILE:
CONSTRUCTION = \$ 175,000
RESURFACE = \$ 16,300
MAINTENANCE = \$ 2,600 (ANNUAL)

NOTE: ESTIMATED COSTS ARE IN CONSTANT 1970 DOLLAR AMOUNTS

Source: SEWRPC.

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Appendix B

A SUMMARY OF PROPOSED ULTIMATE DEVELOPMENT IN THE DELROCK NEIGHBORHOOD

Use	Area (acres)	Percent of Primary Use	Percent of Total Area	Residential Lots	Percent of Lots	Number of Dwelling Units	Percent of Total	Estimated Population	Percent of Total	Estimated School Age Population		Estimated Employment	Percent of Total
										Public	Private		
Residential													
Single-Family	420.0	88.2	44.4	995	93.3	995	58.2	3,484	65.7	898	90	N/A	N/A
Two-Family	23.2	4.9	2.4	71	6.7	142	8.3	443	8.4	115	11	N/A	N/A
Multiple-Family	33.0	6.9	3.5	N/A	—	574	33.5	1,377	25.9	354	35	N/A	N/A
Subtotal	476.2	100.0	50.3	1,066	100.0	1,711	100.0	5,304	100.0	1,367	136	N/A	N/A
Commercial													
Neighborhood Retail and Service	20.5	100.0	2.2	—	—	—	—	—	—	—	—	205	100.0
Community Retail and Service	—	—	—	—	—	—	—	—	—	—	—	—	—
Subtotal	20.5	100.0	2.2	—	—	—	—	—	—	—	—	205	100.0
Industrial	4.6	100.0	0.5	—	—	—	—	—	—	—	—	—	—
Subtotal	4.6	100.0	0.5	—	—	—	—	—	—	—	—	—	—
Government and Institutional													
Public	9.9	86.1	1.0	—	—	—	—	—	—	—	—	—	—
Private	1.6	13.9	0.2	—	—	—	—	—	—	—	—	—	—
Subtotal	11.5	100.0	1.2	—	—	—	—	—	—	—	—	—	—
Park and Recreational													
Neighborhood Parks . . .	10.3	8.0	1.1	—	—	—	—	—	—	—	—	—	—
Community Parks	—	—	—	—	—	—	—	—	—	—	—	—	—
Other Recreational	118.2	92.0	12.5	—	—	—	—	—	—	—	—	—	—
Subtotal	128.5	100.0	13.6	—	—	—	—	—	—	—	—	—	—
Streets and Other													
Public Ways													
Arterial Streets	68.0	24.0	7.2	—	—	—	—	—	—	—	—	—	—
Collector Streets	33.0	11.6	3.5	—	—	—	—	—	—	—	—	—	—
Minor Land	105.3	37.2	11.1	—	—	—	—	—	—	—	—	—	—
Access Streets													
Railroad Right-of-Way .	6.4	2.3	0.7	—	—	—	—	—	—	—	—	—	—
Drainageway and Walkways	70.7	24.9	7.5	—	—	—	—	—	—	—	—	—	—
Subtotal	283.4	100.0	30.0	—	—	—	—	—	—	—	—	—	—
Agricultural, Open Lands, and Unused Lands	20.9	100.0	2.2	—	—	—	—	—	—	—	—	—	—
Subtotal	20.9	100.0	2.2	—	—	—	—	—	—	—	—	—	—
Total	946.0	—	100.0	1,066	100.0	1,711	100.0	5,304	100.0	1,367	136	205	100.0

NOTE: N/A indicates data not applicable.

Gross Neighborhood Density: 6.4 persons per/acre.
2.1 dwelling units per/acre.
Net Neighborhood Density: 11.1 persons per/acre.
3.6 Dwelling unit per/acre.

Source: SEWRPC.

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Appendix C

CITY OF DELAVAN PLAN COMMISSION RESOLUTION ADOPTING THE DELROCK PRECISE NEIGHBORHOOD UNIT DEVELOPMENT PLAN

WHEREAS, the City of Delavan Plan Commission, pursuant to the provisions of Section 62.23 of the Wisconsin Statutes, has the function and duty of making and adopting a master plan for the physical development of the City; and

WHEREAS, the City of Delavan Plan Commission has:

1. Adopted the regional land use and transportation plans for southeastern Wisconsin as prepared by the Southeastern Wisconsin Regional Planning Commission.
2. Prepared and adopted a detailed master plan for land use in the City of Delavan.
3. Prepared and adopted a zoning district map for the City of Delavan.
4. Prepared and adopted an official map ordinance for the City of Delavan.
5. Adopted a plan for the delineation of seven residential neighborhoods for the City of Delavan; and

WHEREAS, the City of Delavan Plan Commission, with the assistance of the staff of the Southeastern Wisconsin Regional Planning Commission, has proceeded to prepare precise plans to guide the future development of one of the seven delineated neighborhoods within the City known as the Delrock Neighborhood, a neighborhood generally bounded by Geneva Street on the north; the new State Trunk Highway 15 on the south, southwest, and southeast; and the Chicago, Milwaukee, St. Paul & Pacific Railroad right-of-way on the northwest; and

WHEREAS, the City of Delavan Plan Commission has held a public informational meeting to acquaint residents and owners within the Delrock Neighborhood with the recommendations contained in the plan as described in SEWRPC Community Assistance Planning Report No. 25; and

WHEREAS, the City of Delavan Plan Commission has considered the plan, together with the statements and requests of individual landowners within the neighborhood, and has proceeded to incorporate, where deemed advisable, their requests in the plan;

NOW, THEREFORE, BE IT RESOLVED THAT:

Pursuant to Section 62.23 of the Wisconsin Statutes, the City Plan Commission on the _____ day of _____, 197_, hereby adopts the precise neighborhood unit development plan described in SEWRPC Community Assistance Planning Report No. 25 as a guide for future development of the Delrock Neighborhood; this plan shall be further deemed to be a part of the master plan of the City of Delavan.

BE IT FURTHER RESOLVED THAT:

The Secretary of the Plan Commission transmit a certified copy of this Resolution to the Common Council of the City of Delavan and the Southeastern Wisconsin Regional Planning Commission.

ATTESTATION:

City of Delavan Plan Commission Chairman

Secretary, City of Delavan Plan Commission

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Appendix D

SUGGESTED OFFICIAL MAP ORDINANCE FOR THE CITY OF DELAVAN, WALWORTH COUNTY, WISCONSIN

SECTION 1. Introduction

WHEREAS, the Common Council of the City of Delavan, after recommendation by the City Plan Commission, has heretofore adopted on _____, 19__, A Jurisdictional Highway System Plan for Walworth County as a guide for community development in the City of Delavan; and

WHEREAS, the City of Delavan and the Southeastern Wisconsin Regional Planning Commission have cooperatively prepared precise neighborhood plans for certain areas of the City, such neighborhoods known as the Delrock Neighborhood, Turtle Creek-North Neighborhood, Turtle Creek-South Neighborhood, Comus Lake-West Neighborhood, Comus Lake-East Neighborhood, Delavan Center Neighborhood, and Industrial/Commercial Neighborhood; and

WHEREAS, the City Plan Commission has recommended to the Common Council that an Official Map Ordinance be established for the City of Delavan; and

WHEREAS, a public hearing was held on _____. 19__, on the question of the adoption of an Official Map Ordinance; and

WHEREAS, the Common Council of the City of Delavan has determined that it is necessary for the proper physical development of the City to establish an Official Map Ordinance for the City of Delavan.

NOW, THEREFORE, the Common Council of the City of Delavan, Wisconsin, do ordain as follows:

SECTION 2. Intent

It is the intent of the Common Council to establish an Official Map for the City of Delavan, Walworth County, Wisconsin, for the purpose of conserving and promoting the public health, safety, convenience, economy, orderliness, and general welfare of the City to further the orderly layout and use of land; to stabilize the location of real property boundary lines; to ensure proper legal descriptions and proper monumenting of land; to facilitate adequate provision for transportation, parks, playgrounds, and storm water drainage; and to facilitate the further subdivision of larger tracts into smaller parcels of land.

SECTION 3. Authority

This Ordinance is enacted under the authority granted by Section 62.23(6) of the Wisconsin Statutes.

¹Please note that this model official map ordinance is intended only as a guide to communities in the formulation of local ordinances. Competent legal, planning, and engineering assistance should be sought in conjunction with the use of this guide by communities in developing local ordinances.

SECTION 4. Jurisdiction

This jurisdictional area of this Ordinance shall include lands in U.S. Public Land Survey Township 2 North, Range 16 East and Township 2 North, Range 15 East, Walworth County, Wisconsin including all of Sections 7, 8, and 18 and portions of Sections 9, 16, 17, 19, and 20 of Township 2 North, Range 16 East, and all of Sections 12 and 13 and portions of Section 24, Township 2 North, Range 15 East.

SECTION 5. Official Map

There is hereby established as the Official Map for the City of Delavan the maps which accompany this Ordinance. All notations, references, and other information shown thereon shall be as much a part of this Ordinance as though the matters and information thereon were fully described herein. These maps shall bear the title "Official Map of the City of Delavan, Wisconsin," their date of adoption or amendment, and their sheet number as recorded in Section 5.3 of this Ordinance. No other map prepared by the City or by private parties shall bear the words "Official Map" upon its face, except as authorized by this Ordinance.

5.1 The Official Map shall consist of ____ map sheets at a scale of 1" = 100'. The City Engineer is authorized to prepare a composite map of all of the Official Map sheets at a smaller scale for the purpose of convenient distribution to the public.

5.2 The Official Map shall show the following information:

5.21 The location and width of all platted and existing streets, highways, drainage-ways, and parkways, and the location and extent of parks and playgrounds within the corporate limits of the City of Delavan as heretofore laid out, adopted, and established by law.

5.22 The location and width of all proposed streets, highways, drainageways, and parkways, and the location and extent of proposed parks and playgrounds as shown in precise neighborhood plans adopted by the City Plan Commission.

5.23 In areas of the City where precise neighborhood plans have not been adopted, the proposed location and width of all streets and highways as shown on A Jurisdictional Highway System Plan for Walworth County.

5.24 Changes and additions as authorized by Section 6 of this Ordinance.

5.3 The following map sheets have been adopted by the Common Council of the City of Delavan, Wisconsin, and are on file with the City of Delavan Clerk and the Walworth County Register of Deeds as required by Sections 10 and 11 of this Ordinance.

<u>Sheet Number</u>	<u>Section Number</u>	<u>Quarter Section</u>	<u>Date Adopted or Amended</u>
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SECTION 6. Changes and Additions

The Common Council may change or add to the Official Map so as to establish the exterior lines of or to widen, narrow, extend, or close any platted, existing, proposed, or planned streets, highways, parkways, parks, or playgrounds.

- 6.1 The Common Council shall refer any change or addition to the Official Map to the City Plan Commission for review and report thereon prior to adoption. The City Plan Commission shall report its recommendation to the Common Council within sixty (60) days.
- 6.2 Changes and additions for the locating, widening, or closing, or the approval of the locating, widening, or closing, of streets, highways, parkways, parks, or playgrounds by the City under provisions of law other than this section shall be deemed to be a change or addition to the Official Map.
- 6.3 A public hearing of parties in interest and citizens before the Common Council shall be required before any changes or additions to the Official Map are effective. Parties in interest are those persons owning land which is to be placed on or removed from the Official Map, abutting property owners, and all property owners within one hundred (100) feet. Notice of the public hearing shall be published as a Class 2 notice pursuant to the requirements of Chapter 985 of the Wisconsin Statutes.
- 6.4 Changes and additions made by duly approved subdivision plats shall not require a public hearing if the changes or additions do not affect any land outside the area being platted.

SECTION 7. Building Permits

For the purpose of preserving the integrity of the Official Map, a building permit shall be required for any structure or part thereof that shall hereafter be located, erected, moved, reconstructed, extended, enlarged, converted, or structurally altered. No permit shall hereafter be issued for any building in the bed of any existing or proposed street, highway, or parkway shown on the Official Map. No permit for the erection of any building shall be issued unless a street, highway, or parkway giving access to such proposed structure has been duly placed on the Map.

The Building Inspector may require each applicant for a building permit to submit a plan, prepared and certified by a registered land surveyor, showing accurately the location of any proposed building with reference to any street, highway, or parkway shown on the Official Map.

SECTION 8. Municipal Improvements

No public sewer or other municipal street utility or improvement shall be constructed in any street, highway, or parkway of the City of Delavan until such street, highway, or parkway is duly placed on the Official Map.

SECTION 9. Appeals

The Board of Zoning Appeals shall have the power to review any administrative decision of the City Building Inspector to deny a permit for the erection of a structure under this Ordinance and to grant relief from the requirements of this Ordinance under the provisions of Section 62.23(6)(d), (f), and (g) of the Wisconsin Statutes.

SECTION 10. Certified Copy of Map

There shall be a certified copy of the Official Map described in Section 5. The certified copy shall be kept in the office of the City Clerk and shall be available for inspection by any interested person during regular office hours. The certified copy shall bear on its face a certification that it is a true copy of the Official Map described in and accompanying this Ordinance and shall show the date of adoption of this Ordinance and shall be signed by the City Mayor and countersigned by the City Clerk. Thereafter, no change or addition to such

Official Map shall become effective until it shall have been indicated by the appropriate convention on the aforesaid certified copy of the Official Map and a certificate placed thereon or attached thereto bearing the number and date of adoption of the amending ordinance. The certificate shall be signed by the City Mayor and countersigned by the City Clerk.

SECTION 11. Map to be Filed with Register of Deeds

The City Clerk shall be responsible immediately upon adoption of the Official Map or any amendment thereto for recording a true copy of the amended Official Map with the Register of Deeds of the County of Walworth, Wisconsin.

SECTION 12. Enforcement

It shall be the duty of the City Building Inspector and the Chief of Police to enforce the provisions of this Ordinance.

SECTION 13. Penalties

Any person, firm, or corporation who fails to comply with the provisions of this Ordinance shall, upon conviction thereof, forfeit not more than two hundred dollars (\$200) and not less than fifty dollars (\$50) and cost of prosecution for each violation, and in default of payment of such forfeiture and costs shall be imprisoned in the county jail until payment thereof but not exceeding thirty (30) days.

No damages shall be allowed for the taking by any governmental agency, for street, highway, and parkway purposes, any building erected in violation of this Ordinance.

SECTION 14. Severability and Conflict

If any section or part of this Ordinance is adjudged unconstitutional or invalid by any court of competent jurisdiction, the remainder of this Ordinance shall not be affected thereby. All other ordinances or parts of ordinances of the City inconsistent with this Ordinance to the extent of the inconsistency only are hereby repealed.

SECTION 15. Effective Date

This Ordinance shall be effective after adoption by the Common Council and publication or posting as provided by law.

Adopted _____

Published _____

Effective _____

Mayor

Countersigned:

City Clerk